

January 17, 2011

SOHO, Historic Preservation Friends in San Diego, and  
Ms. Cathy Winterrowd, Senior Planner, Planning Department [cwinterrowd@sandiego.gov](mailto:cwinterrowd@sandiego.gov)  
City of San Diego Historical Resources Board  
202 C Street, MS-4A  
San Diego, California 92101

Subject: **POTENTIAL ALTERNATIVE DESIGNS – MODIFIED SOHO PLAN** FOR THE  
PLAZA DE PANAMA, CIRCULATION, AND PARKING PROJECT.  
BALBOA PARK, SAN DIEGO, CALIFORNIA.

Dear Fellow San Diegans,

Everyone agrees with the proposed changes to the Plaza de Panama to rid the area of parking, and create a pedestrian square at the center of our public plaza to regenerate Balboa Park. Where people disagree is the Circulation and Parking Elements of the publically funded \$35 million proposed plan to celebrate the Centennial of the Panama-California Exposition of 1915.

<http://www.flickr.com/photos/laplayaheritage/sets/72157624763602387/>

The project leaders and designers held a Public Workshop on January 8, 2011 to receive public input and concerns regarding Circulation and Parking for the Plaza de Panama in time for the 2015 Centennial. The public record of the meeting documented in video and audio; overwhelmingly rejected the Circulation and Parking Elements of the proposed Plan. Figures 1 through 3 depict the proposed Plaza de Panama, Circulation, and Parking Structure designs prepared by the Plaza de Panama Committee. <http://www.signonsandiego.com/news/2011/jan/08/ban-cars-park-bridge-public-says/>

The public had many concerns with the proposed project as it related to historic integrity for National Historic Districts; public funding; the public approval process under the California Environmental Quality Act (CEQA), and the National Environmental Policy Act (NEPA); and timeline for projects subject to the formal approval process for Historic Districts in the National Register of History Places. Many suggestions and design alternatives were presented by Save Our Heritage Organisation (SOHO) and the public. The public also proposed differing policy changes to regenerate the area at virtually no cost to the taxpayers, and without the cost and time to Amend the Balboa Park Master Plan and/or Central Mesa Precise Plan.

[http://www.balboapark.org/sites/default/files/plazadepanama\\_hrb\\_ltr\\_1\\_13\\_11.pdf](http://www.balboapark.org/sites/default/files/plazadepanama_hrb_ltr_1_13_11.pdf)

Though many public concerns and areas of controversy were heard; no suggestions or design alternatives presented by the public at the January 8, 2011 Public Workshop were recognized or incorporated into subsequent plans by the Design Team in this very short amount of time. Although a public controversy exists, design plans have not changed in the recently published Heritage Architecture and Planning (HAP) report to the City of San Diego, Historical Resources Board entitled “*Balboa Park Plaza de Panama, Circulation & Parking Structure Project*,” dated January 13, 2011 (HAP Project No. #10031) (published five days after the Public Workshop). Hopefully these Civic issues can be cleared up, so that the Plaza de Panama Circulation and Parking Project can move forward in a timely manner.

Regards,

Katheryn Rhodes and Conrad Hartsell MD,  
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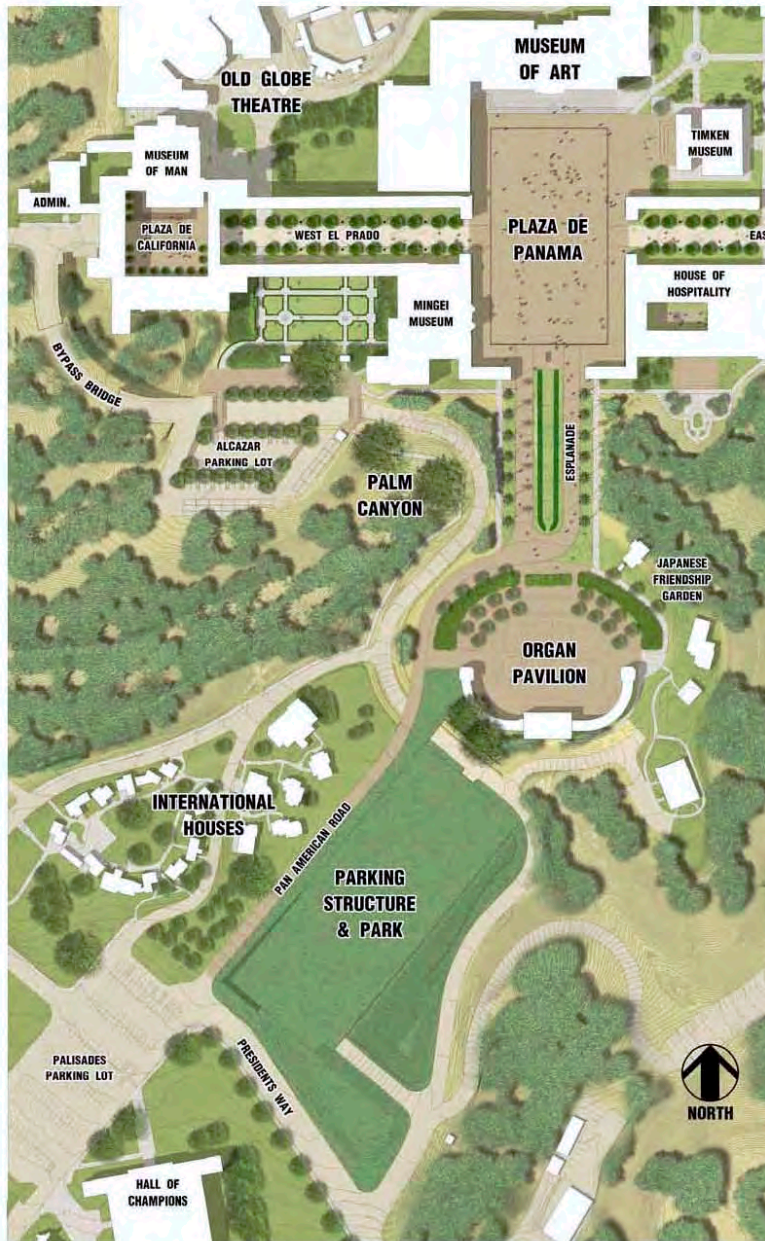


Image 1: Overall concept plan view of the proposed project.

Figure 1 - Image 1 from the HAP report, showing the Plaza de Panama Committee Plan to construct a new Bypass Bridge off the Cabrillo Bridge, reroute traffic just east of Palm Canyon, construct a three-level parking structure with a roof top park land, and divert traffic to the east side of the parking structure. The plan also includes banning through-traffic, and restoring the Plaza de California, El Prado, Plaza de Panama, Esplanade, and the Pan American Road to pedestrian use and park use only.



Image 31: Rendered aerial view of the proposed park atop the parking structure.

Figure 2 - Image 31 from the HAP report, showing the Plaza de Panama Committee Plan to construct a new 3-level parking structure with a roof top park. With thru-traffic on Presidents Way being diverted to the east side of the parking structure, and under Pan American Road. Then thru-traffic would continue on a new paved driveway east of Palm Canyon into the Alcazar Garden Parking Lot. Ending with the Bypass Bridge onto Cabrillo Bridge then Sixth Avenue and Laurel Street.



Image 18: Conceptual bird's eye rendering, looking northeast, showing the bypass bridge and related abutments. This drawing shows the general size and location of the bridge, but the design has not been completed or refined.

Figure 3 – Image 19 from the HAP report, showing the view looking northeast. Including the proposed Bypass Bridge from the Cabrillo Bridge Abutment to the Alcazar Gardens Parking Lot.

*“The entrance was approached across a long bridge across a canyon up to what appears to be a fortified European town” - - the California and Fine Arts building dominate the view.” - Bertham Goodhue*



## **1.0 THE PLAZA DE PANAMA, CIRCULATION, AND PARKING STRUCTURE PROJECT.**

At the Public Workshop on January 8, 2011, the Urban Designer, Mr. Mark Johnson of Civitas Inc. based in Denver, Colorado told the Committee that his design firm would return to the Committee next month in February 2011 with alternative solutions and project designs. After the meeting, Mr. Johnson of Civitas Inc. stated to the press that: “*We’d like to carefully evaluate it and think of some alternatives.*”

It seemed like the Design Team heard the concerns of the public at the Workshop which resulted in many suggestions and inexpensive solutions to preserve the historic integrity, reduce public taxpayer costs, and accomplish the goal of ridding the Plaza de Panama of parking and creating a pedestrian square immediately.

It would be beneficial if the Design Team publically stated their position if they are going to take suggestions from the public and incorporated them into plans, or if the Design Team will be pushing through their initial design for the publically financed Bypass Bridge and 3-level Parking Structure.

[http://www.balboapark.org/sites/default/files/plazadepanama\\_hrb\\_ltr\\_1\\_13\\_11.pdf](http://www.balboapark.org/sites/default/files/plazadepanama_hrb_ltr_1_13_11.pdf)

Subsequent to the January 8, 2011 Public Workshop, the January 13, 2011 HAP Project Summary report was published. In the HAP Report, the project leaders and designers have documented their reasons why their original proposed \$35 million improvements to Circulation and Parking strategies in the Central Mesa of Balboa Park, including a new Bypass Bridge off the east end of the Cabrillo Bridge abutment, are in general compliance with *The Secretary of the Interior’s Standards for Rehabilitation*, and *Standards for the Treatment of Historic Properties*.

Public concerns regarding historic integrity and public costs were minimized in the report, and no mitigation measures are planned. New details in the HAP report include plans for two public restrooms to be removed/destroyed, and not replaced. Including the non-historic archery range restroom at the west end of the Alcazar Gardens parking lot, and the non-historic restroom west of the Spreckles Organ Pavilion. Also the “*charmingly mangy “City Christmas Tree” near Palm Canyon may need to be relocated or replaced.*”

With the removal of parking in the Plaza de Panama, and the reconfiguration of the asphalt parking lot behind the Organ Pavilion into a new three-level Parking Structure with a roof top park, the proposed Parking structure would have a net gain of 264 parking spaces for the Central Mesa Precise Plan.

As such, in order to document inexpensive, alternative public designs and policies; we recommend that SOHO, the Historic Community, and the general public present written designs and alternatives to the Circulation and Parking Project to preserved Balboa Park’s National Historic Landmark (NHL) District status. Approval authorities for the proposed Circulation and Parking Project include the City of San Diego Historical Resources Board, the California State Office of Preservation (State Historic Preservation Officer [SHPO]), and the National Park Services (NPS).

## **2.0 HISTORIC PRESERVATION AND INTEGRITY.**

The importance of the overall historic view of the Cabrillo Bridge and Abutments under the California Environmental Quality Act (CEQA) and the National Environmental Policy Act (NEPA) for Historic Districts in the National Register of History Places should be honored.

Some park proponents are new to the historic Land Use process, which can seem militant to outsiders. Since Urban Designer Mr. Mark Johnson of Civitas is from Denver, Colorado he would only know the historic significance and the need for CEQA/NEPA review from the other Design Team members. Since Mayor Sanders knows the issues within the historic community, the City of San Diego should have included SOHO and the local historic community as an integral part early in the public process on new designs to make fundamental changes to Balboa Park.

Mayor Sanders should have informed the Plaza de Panama Committee about the importance of historic integrity using the 2004 Caltrans Corridor Management Plan as an example of the public Controversy, approval process, and mitigation measure required under law for Nationally-recognized Historic Districts.

Attached in Appendix B please find the Environmental documents prepared by Caltrans in order to add guard rails for public safety to the dangerous State Route 163 through an Updated Corridor Management Plan (CMP). The State Route 163 CMP concluded that guard rails in the center of the freeway would have Potential Significant Environmental Impacts; and the public safety project would include mitigation measures that cost substantially more than the guard rails themselves.

The Plaza de Panama Committee should have been warned about “Potential Significant Environmental Impacts,” the multi-year timeline for approval of controversial projects subject to CEQA and NEPA, and the greater potential for a new Bypass Bridge off the Cabrillo Bridge, compared to new guard rails, “*to change the character of two historic resources: 1) the Balboa Park Historic District; and 2) this portion of Route 163 through Balboa Park.*”

At the Public Workshop, we recommended that the Design Team choose the Save Our Heritage Organisation’s (SOHO) Plan because of the low cost, easy solution, short timeline, and reversibility (see Page 1 of Appendix B). Plus the advantages of the simple public CEQA and NEPA approval processes consisting of a Negative Mitigated Declaration (NMD) instead of the potential for a full Environmental Impact Report/Environmental Impact Statement (EIR/EIS).

To show alternative designs exist, a new alternative low cost designs that incorporates SOHO’s concerns, and Circulation and Parking elements of the Plaza de Panama Committee Plan are presented in Appendix A as the **Modified SOHO Plan**. This is just one alternative design. SOHO, the Historic Community, and the public should write additional alternatives in proposal form so a full range of options are available for the Plaza de Panama Circulation and Parking Project.

Prior to hearing the objections of Save Our Heritage Organisation (SOHO) documented in their presentation entitled, “*Protecting a National Treasure... Bypass Bridge: Design & Impacts to the Cabrillo Bridge and California Building*” we agreed with the original design for a Bypass Bridge designed by the Plaza de Panama Committee.



Now, however, after further analysis we agree with SOHO regarding Historic Integrity concerns for Historic Districts in the National Register of History Places.

<http://sohosandiego.org/images/PlazaDePanamaSOHO.pdf>

The Inventory Nomination Forms and Photographs dated July 19, 1977 to nominate Balboa Park to become a Historic District are attached as Appendix B. The Design Team should study the original National Register forms and nomination photographs to understand why the proposed Bypass Bridge and the supporting columns would directly change the El Prado complex, the Cabrillo Bridge, and the California Building, and should never have been up as a design option for the National Historic District.

See Page 2 of Appendix B for the Inventory Nomination Forms and Photographs depicting the overall historic setting of Balboa Park, the Casa del Prado, the California Building, and the Cabrillo Bridge specifically showing the view where the Bypass Bridge would cross.

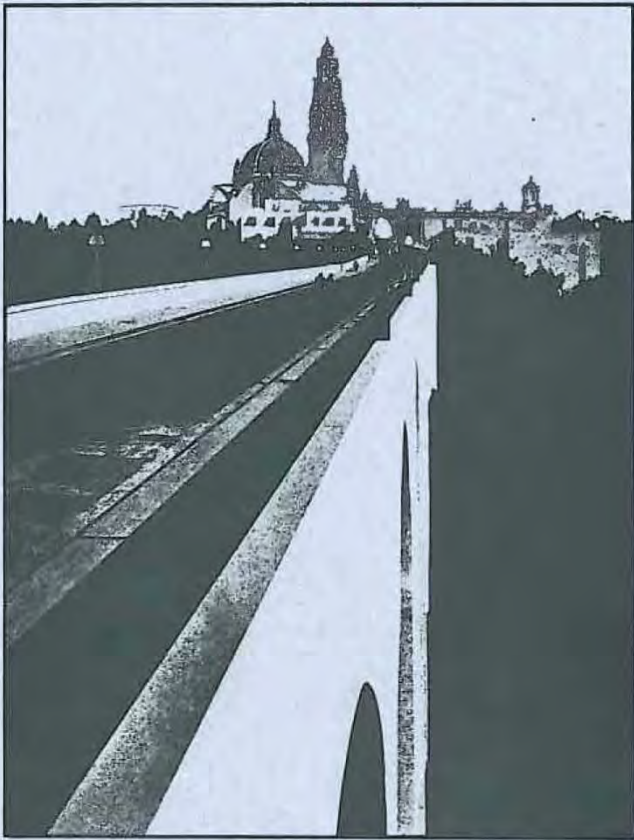
Page 21 states: “A brief description of the grounds at Balboa Park should begin with Bertram Goodhue’s plan: *“The entrance was approached across a long bridge across a canyon up to what appears to be a fortified European town” - - the California and Fine Arts building dominate the view.*”

Page 24 identifies the El Prado complex consisting of the *“Cabrillo Bridge (with approaches and guardhouses).*”

Balboa Park has several Master Plan and Central Mesa Precise Plan Amendments that should be read and understood. The proposed Bypass Bridge off the Cabrillo Bridge Abutment would require Amendments to the Balboa Park Master Plan, and the Central Mesa Precise Plan, which is a much longer public process. If alternative potential project designs conform to the existing Master and Precise Plans, the approval process is accelerated and Plan Amendments are not required.

The HAP report states that *“Preservation advocates have expressed concerns that the new Bypass Bridge will obscure an iconic view of the California Building. It is true that architect Bertram Goodhue emphasized the south and west elevations of the California Building in his early rendering of the Exposition design and several 1915 postcards also feature this portion of the building. Refer to Image 14. However, since the 1930s, this view of the California Building has been completely obscured by a dense wall of trees. Even long-term San Diegans are not aware that these facades exist. In addition, the landform has been re-graded south of the Cabrillo Bridge since 1915. Refer to Images 12 through 19. The reality is the these long-obscured facades will be more visible and appreciated once the Bypass is complete. Not only will the trees will be thinned, but the new views provided from the bypass road will enable the public to see these architectural elements for the first time.”* And *“Visibility of the long-obscured south and west facades of the California Building will be improved.”*

The HAP report included historical views of the Cabrillo Bridge, its eastern Abutment, and the proposed Bypass Bridge into Alcazar Gardens. Figures 4 through 11 show the visual effects of the proposed Bypass Bridge and supporting columns created by the design from the Plaza de Panama Committee, and our interpretation of how the project would create “Potentially Significant Environmental Impacts” that would have the potential to change the character of two historic resources: 1) the Balboa Park Historic District; and 2) the Cabrillo Bridge portion of Route 163 through Balboa Park.

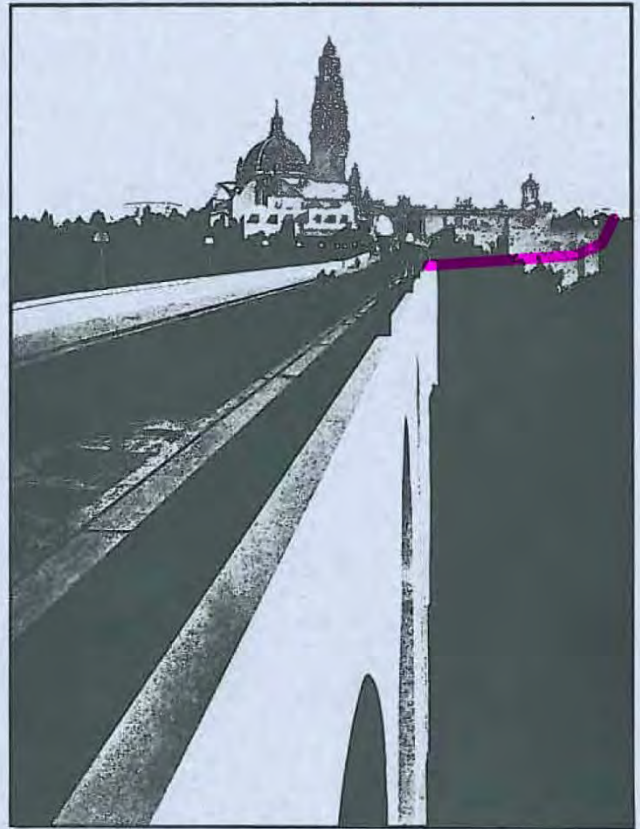


▪ BALBOA PARK ▪

▪ CENTRAL MESA PRECISE PLAN ▪

ADOPTED: October 20, 1992

Figure 4 - Cover Page for the Balboa Park Central Mesa Precise Plan from Goodhue's plan to depict the approach to a "fortified European town."



▪ BALBOA PARK ▪

▪ CENTRAL MESA PRECISE PLAN ▪

ADOPTED: October 20, 1992

Figure 5 - Same historic view of Cover Page including the proposed Bypass Bridge in pink. Note in 1992 the trees were trimmed to show the building facades.



Image 15: A similar view in 2010 with mature landscaping obscuring much of the building. The new bypass bridge would be visible between the trees.

Figure 6 - Image 15 from the HAP report, showing mature trees, but not the view of the proposed Bypass Bridge.

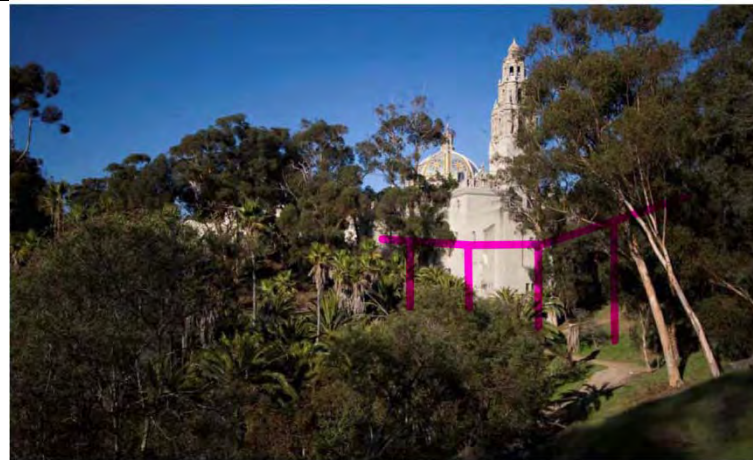
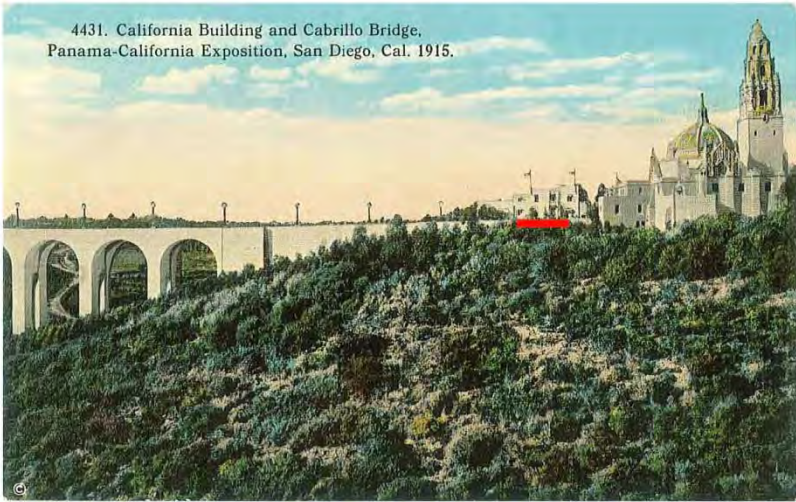
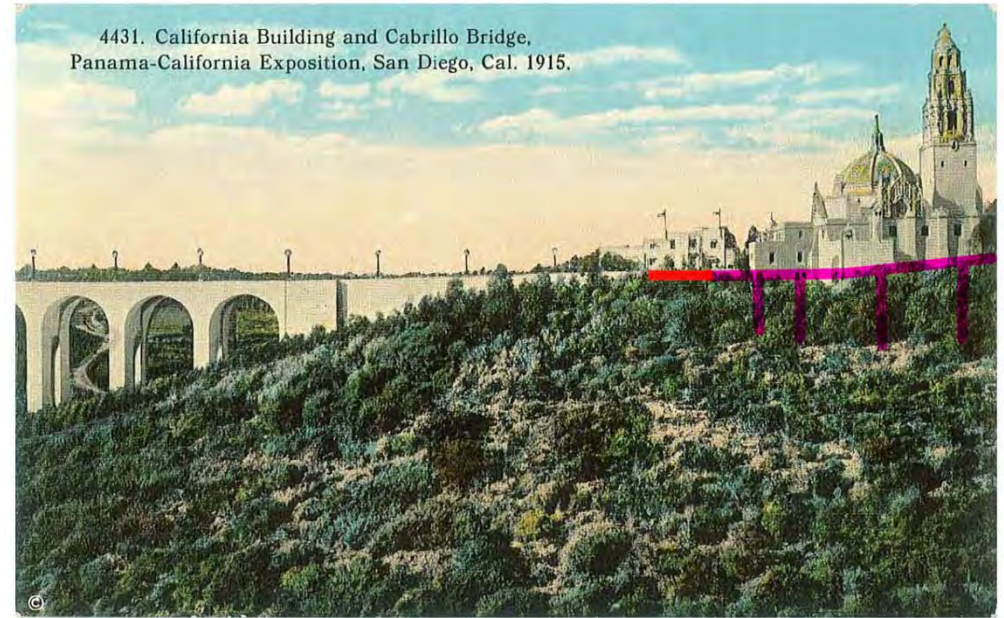


Figure 7 - Same view showing the proposed Bypass Bridge and column supports in pink from Cabrillo Bridge to Alcazar Gardens. Goodhue's plan to depict the approach to a "fortified European town" is interrupted.





**Image 12:** Postcard view of the Cabrillo Bridge and California Building from 1915 with low landscaping. The red line indicates where the bypass will meet the Cabrillo Bridge abutment.



**Figure 9 -** Same view as Image 12 showing the proposed Bypass Bridge and column supports in pink from Cabrillo Bridge to Alcazar Gardens.

**Figure 8 -** Image 12 from the HAP report, showing the location along the Cabrillo Bridge Abutment where changes are proposed in red, but does not include the Bypass Bridge.



**Image 14:** Postcard view of the California Building from 1915 with low landscaping. The Cabrillo Bridge is out of view on the left.



**Figure 11 -** Same view as Image 14 showing the proposed Bypass Bridge and column supports in pink from Cabrillo Bridge to Alcazar Gardens.

**Figure 10 -** Image 14 from the HAP report, showing the historic view of the “fortified European town.” Does not include the Bypass Bridge.



Please note the landscaping and trees in Figures 4 and 5, the photograph in the Cover Sheet of the October 20, 1992 Central Mesa Precise Plan published by the City of San Diego Parks and Recreation Department. The photograph depicts the historic view of the grand entrance to the Central Mesa of Balboa Park from Cabrillo Bridge, with the existing southern building façade depicting a “*fortified European town.*” When this photograph was taken in 1992, the trees in the Archery Range were trimmed back lower than the building elevation. Therefore, the historic views were not blocked, and can be easily return by thinning the trees that block the view.

The HAP report stated: “*Since the 1930s, this view of the California Building has been completely obscured by a dense wall of trees... The reality is the these long-obscured facades will be more visible and appreciated once the Bypass is complete. Not only will the trees will be thinned, but the new views provided from the bypass road will enable the public to see these architectural elements for the first time.*” These statements should be amended to show that the historic view of the California Building has been observed or obscured based upon the recommendations of the City Arborist and maintenance costs. Currently the view is obscured by a forest of trees. The City Arborist should be consulted if the existing trees in the Archery canyon can be trimmed back to expose the southwest corner of the California Building façade to reclaim the historic views as recommended by SOHO.

Ten (10) Standards exist as part of the *Secretary of the Interior’s Standards for Rehabilitation*: <http://www.nps.gov/hps/tps/tax/rhb/index.htm> Standard Nos. 1 through 4 are shown below:

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.

As shown in Figures 4 through 11, we believe the proposed Bypass Bridge would not meet Standard Nos. 1 through 4 of the *Secretary of the Interior’s Standards for Rehabilitation* due to the design elements resulting in the following:

- A large change to the defining characteristics of the building, and its site as an example of a “*fortified European town.*”
- The features and spaces that characterize the site, and the physical record will be permanently altered.

CEQA and NEPA require that all design alternatives and costs be analyzed. Including a “No Project Alternative,” and the alternative, low-cost solutions and policies presented by SOHO and the public during the Public Workshop of January 8, 2011.

### **3.0 CIRCULATION AND PUBLIC PARKING ELEMENTS.**

The ultimate goal of Urban Parks is to activate public spaces for pedestrian and park use only. Principle Circulation Routes in the Balboa Park Master Plan include the Cabrillo Bridge, the Plaza de California, the El Prado, the Plaza de Panama, the Esplanade, and Pan American Road. As stated in the HAP report, the approved Master and Precise Plans with Amendment removed parked cars from the Plaza de Panama. However, the Master and Precise Plans did not “*stem the traffic flow through the West El Prado and Plaza de California.*”

The ultimate goal would be to for park users to arrive via Public Transportation options, in order to rid parkland of at-grade parking, and minimize daily vehicular traffic circulation, as is accomplished in the Central Mesa area during existing Special Events. The Master and Precise Plans also include several transportation alternatives for handicapped person and the elderly, and the use of free public trams throughout Balboa Park.

Vehicular access over the Cabrillo Bridge, and through the Plaza de California, the El Prado, the Plaza de Panama, the Esplanade, and Pan American Road are required for Emergency Vehicles and Fire Lane Access to the Old Globe Theatre, Museum of Man, Museum of Art, the Casa de Prado restaurant, etc. Therefore, these public access roads through the Central Mesa cannot be fully closed and reclaimed for pedestrian use and park use only. As such, vehicular traffic circulation that allows cars to pass through the Plaza de California, the El Prado from the California Building to the Plaza de Panama, then to the Esplanade and Pan American Road have acquired historic significance in their own right through the *Secretary of the Interior's Standards for Rehabilitation*.

At various times throughout the year, especially during annual Special Events like December Nights and the Earth Day Fair, vehicular access over the Cabrillo Bridge, and through the Plaza de California, the El Prado, the Plaza de Panama, the Esplanade, and Pan American Road is suspended and only pedestrian traffic is allowed. These areas can be closed down to traffic at any time creating a Grand ceremonial space and the return of El Prado to pedestrians and park uses at a moment's notice by the use of traffic cones.

Therefore neither a Bypass Bridge nor a paid Parking Structure costing taxpayers approximately \$35 million are needed to accomplish this goal during Special Events. The goal of reclaiming the main axis of the park for pedestrian use and circulation is currently being met during Special Events without costs, and the design is reversible. Creating Traffic Circulation Policies and restriping the roadways, can increase the potential to use the Plaza de Panama as a vital pedestrian square, and can improve the function and appearance of Balboa Park at minimal costs.

The HAP report states that, “*Several times a year, drivers ignore the warning signs and tall trucks strike the underside of the archway bridges causing damage to historic fabric. In 2008, one of the archway bridges required structural repairs after it was hit by a truck.*” Instead of the proposed Bypass Bridge; and at a cost of no money, Balboa Park can create new Traffic Circulation Policies that bans large Truck Traffic from Cabrillo Bridge and the Plaza de California.

The Hap report also states: *“Today, the majority of the Plaza is used for parking (for 67 cars) with vehicle through traffic at the southwest corner. Pedestrian access across the Plaza conflicts with vehicular traffic and the center of the Plaza is cut off from pedestrian use by a traffic circle and parking aisles.”* These statements should be amended to make it clear that currently vehicle traffic, parking, and circulation is everywhere in the Plaza de Panama, not just at the southwest corner.

The HAP report states: *“The proposed new 265,242 square-foot underground parking structure would provide 785 parking spaces on three levels with a 97,000 square-foot rooftop park. This results in a net gain of 264 parking spaces for the Central Mesa... While some have never supported parking in this location, referring to it as the “core” of the park, there is a strong consensus that this is the right location, between the two historic districts, and it does not negatively impact Balboa Park... Because of the costs associated with the construction of an underground parking facility, as well as the utility, labor, and maintenance demands, it is likely that paid parking will be implemented for this new parking structure.”*

During the January 8, 2011 Saturday Public Workshop many members of the public stated that they do not support a paid parking structure behind the Speckles Organ Pavilion. There is not a strong consensus that a parking structure is needed for a net gain of 264 parking spaces. Also the public was adamant that parking should remain free in Balboa Park, and the publically financed project should be constructed at little to no costs to taxpayers.

#### **4.0 CONCLUSION.**

Project leaders and designers have yet to acknowledge the need to change their plans based upon public concerns regarding historic integrity, costs to publically finance the improvements, the CEQA/NEPA approval process, and the timeline.

Hopefully project leaders, park designers, the City of San Diego, the Parks and Recreation Department, and the Historical Resources Board, will understand the concerns of SOHO and the public, and incorporate the low-cost design solutions and circulation and parking policies into project plans in time for the Centennial of the 1915 Panama-California Exposition in Balboa Park.

Thank you to the Plaza de Panama Committee and philanthropic San Diego leaders for putting their project and design ideas forward, so this important Civic discussion can continue on improvements for Circulation and Parking within the Central Mesa of Balboa Park. We are confident that solutions that are acceptable to all parties can be found to create a wonderful pedestrian experience within the Plaza de Panama.



**APPENDIX A – MODIFIED SOHO PLAN.**

We are advocating for an alternative design and solution for traffic circulation and parking in the Central Mesa area of Balboa Park in San Diego, California that we call the **Modified SOHO Plan**. The Plan is based upon the Save Our Heritage Organisation’s (SOHO) plan to rid the Plaza de Panama of parking, and reroute traffic through the southwest corner of the Plaza as shown in Figures A-1 to A-7, and the photographs on Page 1 of Appendix B.

<http://www.signonsandiego.com/news/2011/jan/08/ban-cars-park-bridge-public-says/>

*“Many of the speakers endorsed an alternative advocated by the [Save Our Heritage Organisation](#) that would involve continuing traffic across the bridge but routed along the southwest edge of the plaza in front of the House of Charm, home to the [Mingei International Museum](#). Parking in the plaza would disappear and, in a second phase, traffic might be banned altogether.”*

The Plaza de Panama Committee has a great idea to eliminate *“the traffic congestion at the intersection of Pan American Road and Presidents Way, and shifting the vehicular interchange to the east, the current intersection is abandoned allowing for the future removal of parking from Pan American Plaza.”* As part of the **Modified SOHO Plan** we are incorporating the shifting of vehicular traffic to the asphalt parking lot behind the Speckles Organ Pavilion, and routing traffic through the east side of the parking lot, without the need for a new parking structure or grade separation under Pan American Road to Presidents Way. . Thus saving taxpayers millions of dollars we do not have, and saving public funds for a new and improved Balboa Park Tram System that everyone agrees is needed. Minimal grading would be required for an entrance from traffic northbound on the Esplanade into the Alcazar Garden Parking Lot, and an entrance and intersection to connect Pan American Road to the existing asphalt parking lot south of the Spreckles Organ Pavilion. Figures A-1 through A-4 depict the proposed changes from the Plaza de Panama Committee plan compared to the **Modified SOHO Plan**.



**Image 7:** Conceptual bird’s eye rendering of the rehabilitated Plaza de Panama, looking southeast, showing the option of new trees. (Note: The paving grid will not be done.)



Figure A-2. Same view as Image 7 showing SOHO’s alternative plan to reroute two-way traffic through El Prado and the Southwest corner of the Plaza de Panama in pink. Thus freeing the Plaza de Panama of parking, and the majority of traffic circulation, at virtually no cost, except to restripe traffic lanes, and not add trees.

Figure A-1 - Image 7 from the HAP report, depicting the removal of parking spaces, and the removal of traffic circulation in the Plaza de Panama, and El Prado. The design would create a pedestrian-friendly public Plaza at the heart of Balboa Park.



**Image 23:** View of the Esplanade in 2010, looking north.

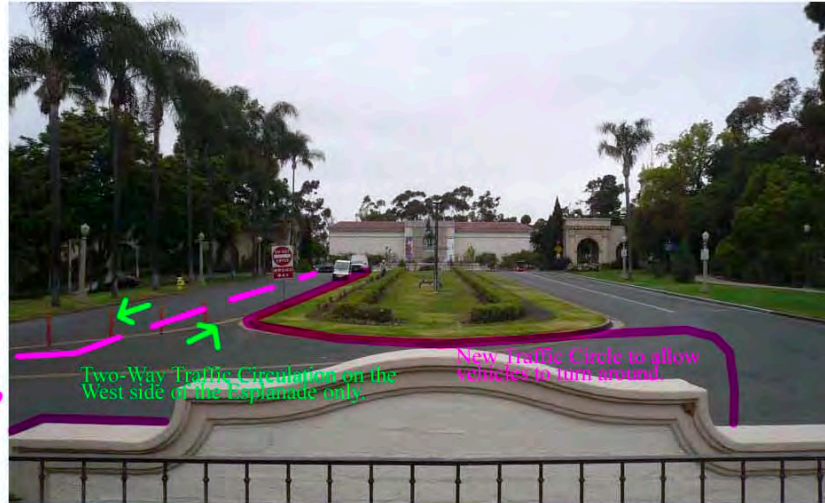


Figure A-4. Same view as Image 23 showing the alternative **Modified SOHO Plan** to reroute two-way traffic through the west side of the Esplanade in pink. Thus freeing the east side of the Esplanade of traffic circulation to encourage a safe pedestrian experience. Costs includes funding to restripe traffic lanes, and included a new traffic circle to allow passenger drop off and vehicles to turn around, west of the Japanese Friendship Garden entrance.

Figure A-3 - Image 23 from the HAP report, showing the current traffic circulation for the Esplanade from the Spreckles Organ Pavilion to the Plaza de Panama.



Figure A-5 – Room for a new Entrance to the Alcazar Garden Parking Lot for handicap parking, Valet service, and drop off location for passengers from the northbound lane of the west Esplanade. Minimum grading is required and should cost less than \$10,000.



Figure A-6 – The exit from the Alcazar Garden Parking Lot would stay the same. Thus the “*charmingly mangy City Christmas Tree*” near Palm Canyon, and the public restrooms west of the Spreckles Organ Pavilion will not have to be altered or destroyed. Thus saving taxpayer money.





Figure A-7 – Modified SOHO Plan with features of the Plaza de Panama Committee Plan showing two-way access through the Plaza de California, El Prado, the southwest corner of the Plaza de Panama, the west Esplanade, and portions of Pan American Road. Including a new vehicular traffic circulation pattern and two way lanes behind the Spreckles Organ Pavilion, moving through-traffic to the east side of the existing parking lot. Thus eliminating “the traffic congestion at the intersection of Pan American Road and Presidents Way, and shifting the vehicular interchange to the east, the current intersection is abandoned allowing for the future removal of parking from Pan American Plaza.”



Our Recommendation is to choose the Save Our Heritage Organization’s (SOHO) Plan, because of the low cost, easy solution, and simple public CEQA process. Do the planned improvements in 3 phases.

Phase 1 - SOHO's inexpensive idea to reroute traffic through the southwest corner of the Plaza. The area for two-way through traffic should take up less than 15 percent of the total area of the Plaza de Panama. The cost to restripe traffic patterns and minimal grading into Alcazar Gardens should cost approximately \$10,000 with no replacement parking. Eliminate parking in the plaza immediately to get San Diegans use to the idea.

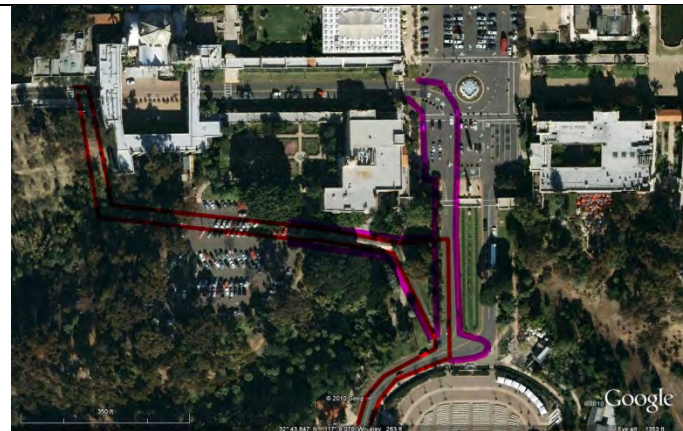
Phase 2 - Establish a public parking and alternative regional Transportation strategies for Special Events from the free public Tailgate Parking lot in downtown and the County Administration public parking lots. Inside the park transportation alternatives should include in-park trams, free elderly transportation using solar powered Golf Carts run by volunteers, and the planned Park to Park shuttle, from North Park to Petco Park and the trolley.

Phase 3 - If needed, re-examine SOHO's simple, inexpensive, and reversible strategy after 6 months of use. If unacceptable, then a Cost/Benefit Analysis will be completed, and rethink about the new \$35 million Bypass Bridge and paid parking structure.

Regards, Katheryn Rhodes and Conrad Hartsell MD 619-523-4350 [rhodes@laplayaheritage.com](mailto:rhodes@laplayaheritage.com)

PROS: Less cost. Reversible. Phased. Construction can be completed in less than one week. Easy CEQA and Historical review and approval process. Parking can be eliminated in the Plaza immediately.

CONS: Takes 6 months out of the process to get rid of parking cars in the Plaza de Panama before the 2015 deadline of Balboa Park’s Centennial Celebration.



SOHO’s plan in pink. Easy to reroute traffic through the southwest corner of the Plaza de Panama and eliminate parking, with no new parking spots. Cost should be less than \$10,000.

Mr. Jacobs’ plan in red for a \$35 million dollar By-Pass Bridge and paid parking garage behind the Spreckles Organ Pavilion, with park land on top of parking structure.



Alcazar Gardens Parking lot would become a drop off and turn around area with Handicapped, Valet, and short-term 15 minute parking.

The actual cars that are dropped off for paid Valet parking will still be parked behind the Spreckles Organ Pavilion.

The entrances and exits to the Alcazar Gardens parking lot will be reconfigured.





Ohio Statehouse Credit: Jeffrey Darbee

BALBOA PARK.





**BALBOA PARK HISTORIC DISTRICT**

San Diego, California

PHOTO CREDIT: City of San Diego

DESCRIPTION: Cabrillo Bridge, View  
from the South



BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: House of Charm, North  
Portal



Hall of  
Champions





BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: Fine Arts Gallery, View  
from the South



BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: Natural History Museum,  
View from South





BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: Casa del Prado, view from  
South



BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: House of Hospitality,  
View of the West





Hotel El Prado Club  
Alcázar Casino

BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: The El Prado Arcade



BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: Casa del Prado, view from South.





BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: Organ Pavilion, View from  
Northwest



BALBOA PARK HISTORIC DISTRICT  
San Diego, California  
PHOTO CREDIT: City of San Diego  
DESCRIPTION: Electrical Building, View  
from the North









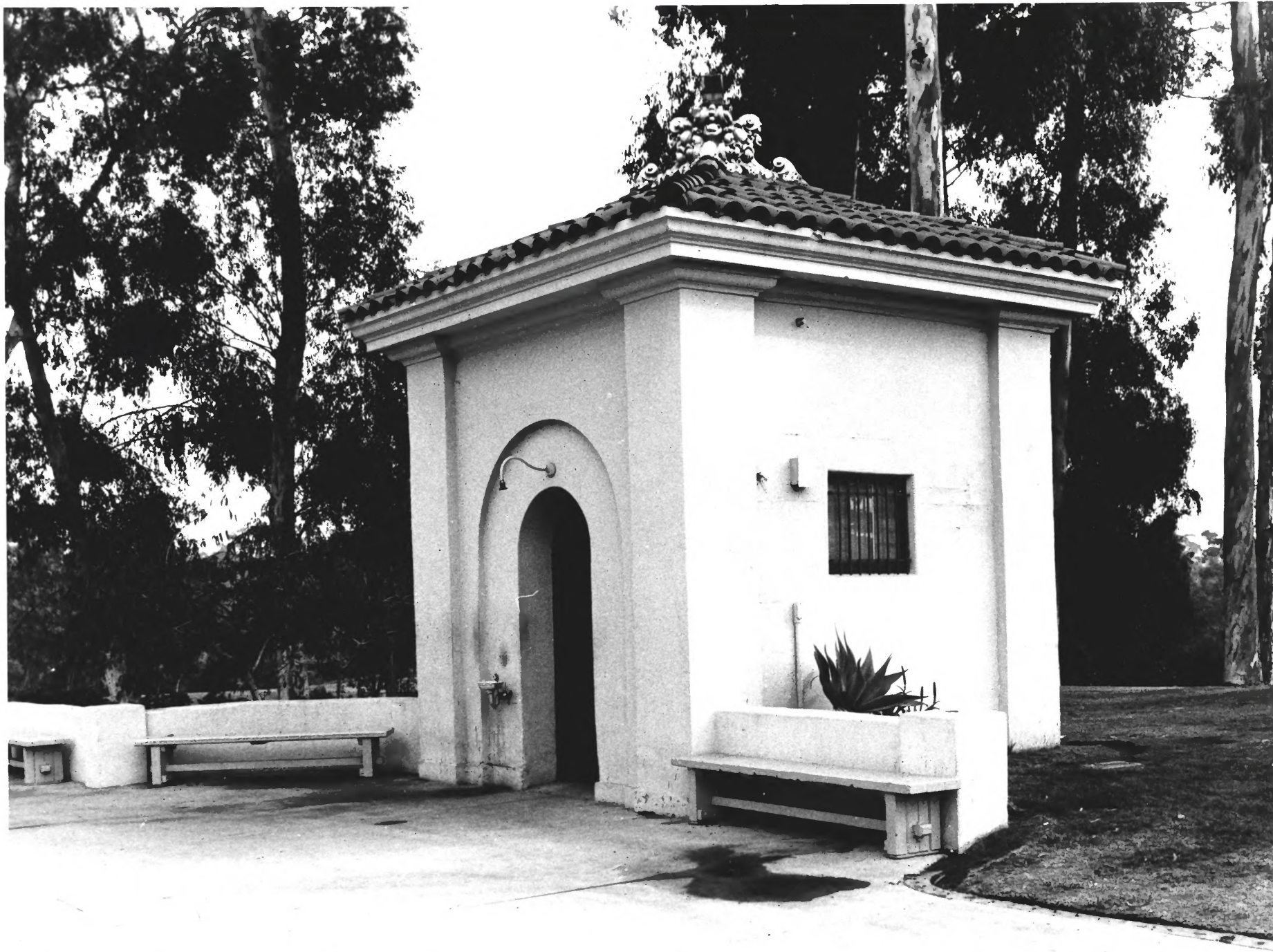
BALBOA PARK HISTORIC DISTRICT

San Diego, California

PHOTO CREDIT: HABS, Marvin Rand

DESCRIPTION: Balboa Park, Entrance





Cabrillo Bridge 1A.  
West Approach - Guardhouse  
on South Side of El Prado

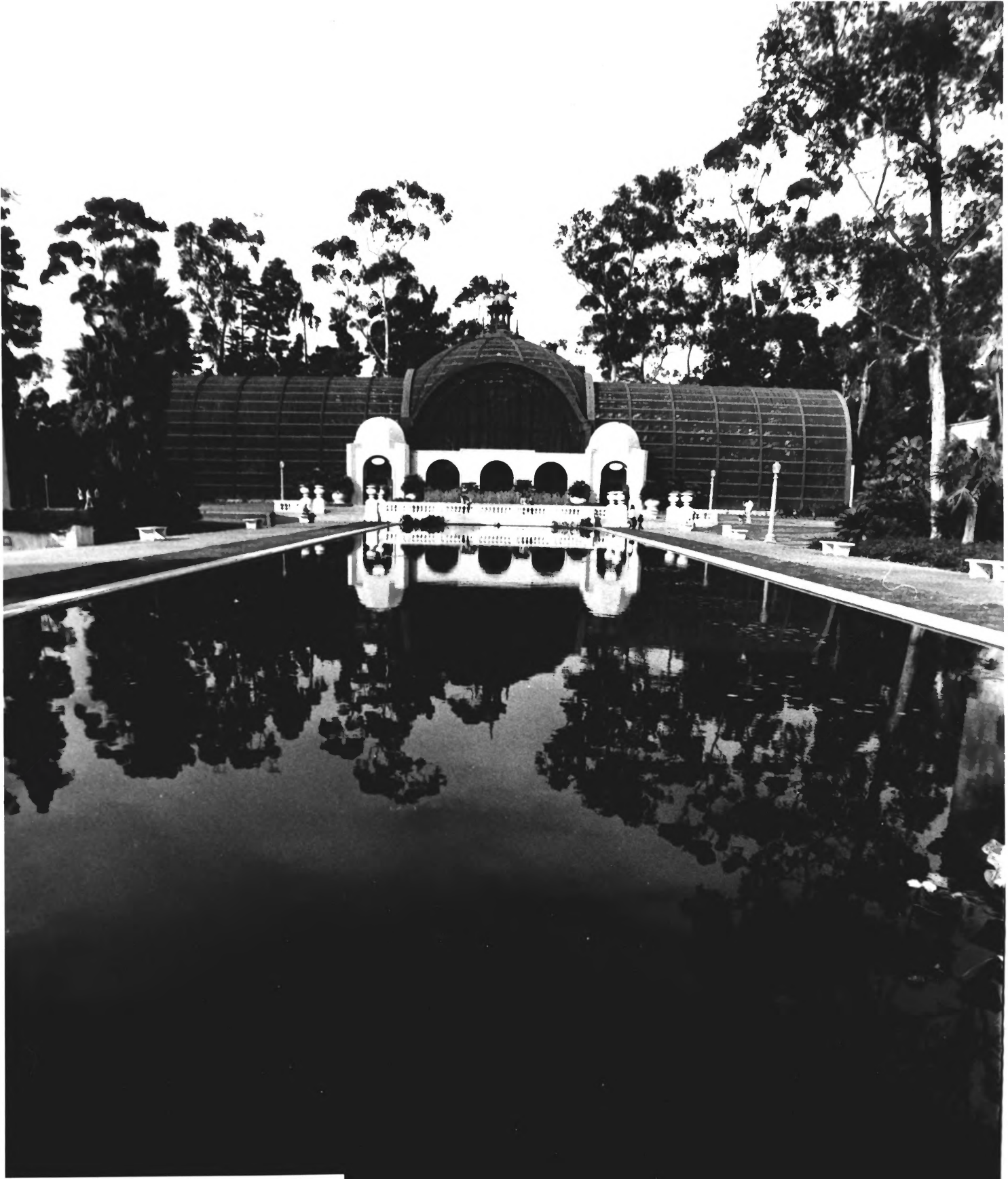




House of Charm  
View from the East

2.





Botanical Building  
View from the South

5.







Fine Arts Gallery  
View from the South

10A.

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**NATIONAL REGISTER OF HISTORIC PLACES  
INVENTORY -- NOMINATION FORM**

SEE INSTRUCTIONS IN *HOW TO COMPLETE NATIONAL REGISTER FORMS*  
TYPE ALL ENTRIES -- COMPLETE APPLICABLE SECTIONS

**1 NAME**

HISTORIC

Balboa Park

AND/OR COMMON

**2 LOCATION**

STREET & NUMBER

California Quadrangle 41

— NOT FOR PUBLICATION

CITY, TOWN

CONGRESSIONAL DISTRICT

San Diego

— VICINITY OF

42nd

STATE

California

CODE

06

COUNTY

San Diego

CODE

073

**3 CLASSIFICATION**

**CATEGORY**

- DISTRICT
- BUILDING(S)
- STRUCTURE
- SITE
- OBJECT

**OWNERSHIP**

- PUBLIC
- PRIVATE
- BOTH
- PUBLIC ACQUISITION**
- IN PROCESS
- BEING CONSIDERED

**STATUS**

- OCCUPIED
- UNOCCUPIED
- WORK IN PROGRESS
- ACCESSIBLE**
- YES: RESTRICTED
- YES: UNRESTRICTED
- NO

**PRESENT USE**

- AGRICULTURE
- COMMERCIAL
- EDUCATIONAL
- ENTERTAINMENT
- GOVERNMENT
- INDUSTRIAL
- MILITARY
- MUSEUM
- PARK
- PRIVATE RESIDENCE
- RELIGIOUS
- SCIENTIFIC
- TRANSPORTATION
- OTHER.

**4 OWNER OF PROPERTY**

NAME

City of San Diego

STREET & NUMBER

City Hall, 202 C Street

CITY, TOWN

San Diego

— VICINITY OF

STATE

California

**5 LOCATION OF LEGAL DESCRIPTION**

COURTHOUSE,  
REGISTRY OF DEEDS, ETC

County Recorder's Office

STREET & NUMBER

1222 First Street

CITY, TOWN

San Diego

STATE

California

**6 REPRESENTATION IN EXISTING SURVEYS**

TITLE

City of San Diego Site Board Register #1

DATE

1967

— FEDERAL — STATE — COUNTY  LOCAL

DEPOSITORY FOR  
SURVEY RECORDS

City Administration Building--Planning Department

CITY, TOWN

San Diego

STATE

California



## 7 DESCRIPTION

CONDITION		CHECK ONE	CHECK ONE
<input type="checkbox"/> EXCELLENT	<input type="checkbox"/> DETERIORATED	<input type="checkbox"/> UNALTERED	<input checked="" type="checkbox"/> ORIGINAL SITE
<input checked="" type="checkbox"/> GOOD	<input type="checkbox"/> RUINS	<input checked="" type="checkbox"/> ALTERED	<input type="checkbox"/> MOVED      DATE _____
<input type="checkbox"/> FAIR	<input type="checkbox"/> UNEXPOSED		

---

### DESCRIBE THE PRESENT AND ORIGINAL (IF KNOWN) PHYSICAL APPEARANCE

A brief description of the grounds at Balboa Park should begin with Bertram Goodhue's plan: "The entrance was approached across a long bridge across a canyon up to what appears to be a fortified European town--the California and Fine Arts building dominate the view. Inside the grounds the exposition was laid out along the major axis of El Prado with the minor axis of the Plaza de Panama intersecting it. The California Quadrangle provided a primary open space at the entry, and small courts and vistas into courtyards with reflecting pools, which break the force of the main axis. Buildings and colonnades encircle and line these spaces which encouraged observers to compare it with a Spanish town. In its layout, however, the plan was highly classical, while the buildings, especially the main ones by Goodhue, gave the place its character.

The sense of homogeneity was in part due to the planting materials, for a variety of architectural styles were present, including classical, Spanish colonial, and pueblo. Nevertheless, the California Building and the Fine Arts Building, flanking the California Quadrangle to provide the dramatic view on the approach to the grounds, established a feeling of ensemble for the layout and the stylistic character of the architecture."<sup>1</sup>

The California Building and the Fine Arts Building were intended to be permanent structures and are not only distinguished architecturally but are constructed in reinforced concrete, with terra cotta infill and covered with stucco. There is an overlay of very complicated decorative ornament and cast stone sculpture designed by the Piccirilli Brothers of New York. The on-site architectural work was done by Carleton W. Winslow, Sr.

The California Building strongly resembles a baroque-Spanish church embellished with plaster ornament with a glazed tile dome which glitters in the sunlight. The sculpture represents the history of California and its Spanish origins. The building design is derived from the church of San Diego in Guanojuto in Mexico. The tile work was executed by Walter Nordhoff of San Diego. There are elaborate architectural descriptions of all buildings in Florence Christman's, "The Romance of Balboa Park."

Nearby is the other large structure in this group--the Fine Arts Building, it stands with connecting quadrangle walls to the California Building. It houses the Chapel of St. Francis and the "Wonder of Life" exhibit. The chapel contains a richly sculptured altarpeice with Saints associated with the history of Spain, Mexico and California. Although the chapel is used for ceremonies and weddings it has never been consecrated.

(Continued)

**8 SIGNIFICANCE**

PERIOD	AREAS OF SIGNIFICANCE -- CHECK AND JUSTIFY BELOW			
<input type="checkbox"/> PREHISTORIC	<input type="checkbox"/> ARCHEOLOGY-PREHISTORIC	<input type="checkbox"/> COMMUNITY PLANNING	<input checked="" type="checkbox"/> LANDSCAPE ARCHITECTURE	<input type="checkbox"/> RELIGION
<input type="checkbox"/> 1400-1499	<input type="checkbox"/> ARCHEOLOGY-HISTORIC	<input type="checkbox"/> CONSERVATION	<input type="checkbox"/> LAW	<input type="checkbox"/> SCIENCE
<input type="checkbox"/> 1500-1599	<input type="checkbox"/> AGRICULTURE	<input type="checkbox"/> ECONOMICS	<input type="checkbox"/> LITERATURE	<input checked="" type="checkbox"/> SCULPTURE
<input type="checkbox"/> 1600-1699	<input checked="" type="checkbox"/> ARCHITECTURE	<input checked="" type="checkbox"/> EDUCATION	<input type="checkbox"/> MILITARY	<input type="checkbox"/> SOCIAL/HUMANITARIAN
<input type="checkbox"/> 1700-1799	<input checked="" type="checkbox"/> ART	<input type="checkbox"/> ENGINEERING	<input type="checkbox"/> MUSIC	<input type="checkbox"/> THEATER
<input type="checkbox"/> 1800-1899	<input type="checkbox"/> COMMERCE	<input type="checkbox"/> EXPLORATION/SETTLEMENT	<input type="checkbox"/> PHILOSOPHY	<input type="checkbox"/> TRANSPORTATION
<input checked="" type="checkbox"/> 1900-	<input type="checkbox"/> COMMUNICATIONS	<input type="checkbox"/> INDUSTRY	<input type="checkbox"/> POLITICS/GOVERNMENT	<input type="checkbox"/> OTHER (SPECIFY)
		<input type="checkbox"/> INVENTION		

SPECIFIC DATES 1915, 1935 BUILDER/ARCHITECT Bertram Goodhue, Carleton Winslow, Sr, Richard S. Regua, Walter Dorwin Teague

STATEMENT OF SIGNIFICANCE

Balboa Park is the cultural center of San Diego as well as being a beautifully designed urban area--one of the best planned and landscaped in America. The buildings are some of the finest Spanish Baroque revival architecture extant.

John Henderson describes the history of the park:

"In 1868, the board of trustees set aside 1400 acres of undeveloped, city owned pueblo lands for a park. This dedication was perhaps the greatest single act of foresight in San Diego's history. For many years following, the site was a park in name only as its barren hills and valleys were used only for cattle grazing, dumping grounds and gravel digging. An Indian Rancheria and city dog pound were located here and only the wildflowers gave some aesthetic relief. Constant encroachment threatened to reduce the park acreage and several attempts to sell off portions were successfully thwarted by citizens groups. However, the gradual erosion occurred with acreage given to Russ School (1881), the Children's Home (1887) and an orphanage (1889) which later became the Naval Hospital. Public interest in park beautification began in 1889 with tree plantings and a few years later, the establishment of a nursery by Kate O. Sessions. Her "rental" agreement included "planting 100 trees each year in City Park and donating another 300 trees each year for other city owned parks." To this great woman, the city owes a debt for her introduction of the cypress, torrey pine, jacaranda, cork oak, pepper, eucalyptus, fan palm and many other trees which now grace the park and various parts of the city.

In 1902, the major turning point, George W. Marston commissioned Samuel Parsons Jr., New York landscape architect, to prepare a master plan for the park. Much of the basic road, path and tree development we see today is the result of the implementation of this plan. By 1909, the idea of an exposition was formulated to celebrate the opening of the Panama Canal, scheduled for completion in 1915. Civic leaders, led by G. Aubrey Davidson, (president of the Chamber of Commerce), rallied around the tremendously ambitious project and hired Bertram Grosvenor Goodhue from New York as the architect. Originally envisioned by the Director General (Col. D. C. Collier) as a miniature Mission-style city it is interesting speculation to wonder what role Irving Gill played in the early planning. Certainly unmistakably Gill motifs occur

(Continued)



# 9 MAJOR BIBLIOGRAPHICAL REFERENCES

(See continuation sheet).

# 10 GEOGRAPHICAL DATA

ACREAGE OF NOMINATED PROPERTY \_\_\_\_\_

UTM REFERENCES

A	1,1	48,639,0	362,18,3,0	B	1,1	48,638,0	362,11,5,0
	ZONE	EASTING	NORTHING		ZONE	EASTING	NORTHING
C	1,1	48,560,0	362,10,5,8,0	D	1,1	48,520,0	362,11,3,6,0
	ZONE	EASTING	NORTHING		ZONE	EASTING	NORTHING

VERBAL BOUNDARY DESCRIPTION

(See continuation sheet).

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE	CODE	COUNTY	CODE
STATE	CODE	COUNTY	CODE

# 11 FORM PREPARED BY

NAME / TITLE

Ms. Carolyn Pitts, Architectural Historian

ORGANIZATION

Historic Sites Survey Division, NPS

DATE

7/19/77

STREET & NUMBER

1100 L Street, NW.

TELEPHONE

202-523-5464

CITY OR TOWN

Washington,

STATE

D.C.

# 12 STATE HISTORIC PRESERVATION OFFICER CERTIFICATION

THE EVALUATED SIGNIFICANCE OF THIS PROPERTY WITHIN THE STATE IS:

NATIONAL \_\_\_\_

STATE \_\_\_\_

LOCAL \_\_\_\_

As the designated State Historic Preservation Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service.

FEDERAL REPRESENTATIVE SIGNATURE

TITLE

DATE

FOR NPS USE ONLY

I HEREBY CERTIFY THAT THIS PROPERTY IS INCLUDED IN THE NATIONAL REGISTER

DATE

DIRECTOR, OFFICE OF ARCHEOLOGY AND HISTORIC PRESERVATION  
ATTEST:

DATE

KEEPER OF THE NATIONAL REGISTER

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The other structure in this California Quadrangle complex is the Administration Building which was the first one to be completed for the 1915 fair. It was designed by Carleton Winslow, Sr. It stands, like a guardhouse, to the north of the West Gate. There is crisp ornamentation on the doors and windows in the Moorish style.

The El Prado Complex consists of the following structures, buildings, gardens and plazas:

The Cabrillo Bridge (with approaches and guardhouses)

The House of Charm

The House of Hospitality

The Electrical Building

The Botanical Building

The Organ Pavilion

The Alcazar Gardens

The Plaza de Panama

The El Prado Arcade

The Fine Arts Gallery

The Casa del Prado (reconstruction)

The Natural History Museum

The following brief description is extracted from Jean Stern's National Register report.

House of Charm

The House of Charm is a two story building located on the southwest corner of the Plaza de Panama. It is designed in the relatively plain "mission" style, with very little elaborate ornamentation.

The main entrance to the House of Charm is on the east side of the building, facing onto the Plaza de Panama. The facade juts out from the side of the building, and is highlighted by two large engaged towers flanking the main doorway.

The House of Charm was designed by Carleton Monroe Winslow. It was originally built as a temporary building for the 1915 Panama-California Exposition.

(Continued)



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House of Hospitality

The House of Hospitality is a two story building located on the southeast corner of the Plaza de Panama. It is based upon the highly ornamented Spanish Baroque style, with heavy emphasis on door and window moulding.

The northwest corner of the building is embellished by a tall square tower, the upper part of which is covered with very elaborate detailed relief.

The south side of the building is characterized by an open-air patio area and a series of terraces leading south into a garden.

The interior of the House of Hospitality is an open two-story courtyard. Both stories are arcaded and supported by columns and pillars on each level. In the center is an octagonal tiled fountain with a statue.

The House of Hospitality was originally designed as a temporary building for the 1915 Panama-California Exposition. At that time, it was called the Foreign Arts Building and it had a south wing and no central open courtyard. The building was saved from destruction and, with alterations, was used during the 1935 California Pacific International Exposition, where it was called the House of Hospitality. The 1935 alterations were the removal of the south wing to make way for the patio and terraces, and the removal of the central part of the building where the courtyard now exists.

The original design is by Carleton Monroe Winslow, and the 1935 alterations are by Sam Hamill.

Electrical Building

The Electrical Building is a two story building located on the south side of El Prado, just east of the House of Hospitality. The building is laid out on an east-west axis, with two pavilions extending out of the main (north) face.

The north face is highly decorated, with two main entrances, each within a pavilion, one near the northeast corner, the other near the northwest corner. The first story (street level) is lined by the extension of the El Prado arcade. The street side of

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the arcade is open, each arch supported by a rectangular pillar. The building side of the arcade joins into the building wall, creating a blind arcade. The arcade changes as it passes through both pavilions by becoming elaborately decorated vestibules trimmed with several rows of moulding.

The Electrical Building was designed by Frank P. Allen, Jr. It was originally built as a temporary building for the 1915 Panama-California Exposition. At that time it was called the Commerce and Industries Building. In 1916, the name was changed to the Canadian Building. The building was saved from destruction and during the 1935 California Pacific International Exposition it was called the Palace of Better Housing.

The elaborate Spanish Baroque design of the Electrical Building was inspired by the Casa Consistorial at Palma de Majorca, Spain.

Botanical Building

The Botanical Building is a large lath covered structure housing an interesting collection of plants. The main portion of the building is built of redwood, supported by steel trusses painted to match the redwood. The overall shape of the building is that of a central dome of open-work lath, with two barrel vaults extending east and west, also of open-work lath. The central dome rests on four large steel arches covered in redwood. The dome is capped by an open cupola.

The Botanical Building was designed by Carleton Monroe Winslow. It was built for the 1915 Panama-California Exposition. At that time, it had a northern wing which extended from the central dome. This wing was different from the east and west wings in that it was glass-covered. The building remained virtually intact into the 1935 California Pacific International Exposition, where it housed a botanical display.

Organ Pavilion

The Organ Pavilion is located south of the Plaza de Panama, along Pan American Road. It is a building over three stories tall, flanked on the east and west by two colonnades, which curve out in front of the building (north side) forming a large atrium. In front of the building is a concrete stage that is completely open-air, as is the atrium.

(Continued)



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The central building contains the organ. The facade of the building has a huge arch in the center.

The building is framed at all four corners by monumental engaged columns with spiraling vines. These are topped by Corinthian capitals, upon which is a winged angel blowing twin long trumpets.

The bronze plaque on the west side of the central arch on the front of the building reads as follows:

To the people of San Diego this pavilion and organ are presented and to them and the people of all the world this pavilion and organ are dedicated by John D. Spreckels and Adolph B. Spreckels, January First A.D. Nineteen Hundred and Fifteen. Harrison Albright Architect.

Alcazar Gardens

The Alcazar Gardens lie between the California Quadrangle and the **House of Charm**, on the south side of El Prado. The gardens are layed out in a rectangular form, with the east-west axis being longer.

El Prado Arcade

The south side of El Prado is lined by a continuous arcade that originates at the California Quadrangle, continues around the south side of the Plaza de Panama, and terminates at the northeast corner of the Electrical Building.

The Alcazar Gardens were designed by Frank P. Allen, Jr. They were built for the 1915 Panama-California Exposition, where they were known as Los Jardines de Montezuma. At that time, the semi-circular apse at the west end was topped by a pergola. Later, the gardens were re-designed for the 1935 California Pacific International Exposition, and were named the Alcazar Gardens. In 1962, restoration of the garden was sponsored by the San Diego Rotary Club.

Plaza de Panama

The Plaza de Panama is formed by the intersection of El Prado with Pan American Road, the central north-south avenue which begins at the Plaza de Panama and leads

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southward to the Organ Pavilion and into the Palisades Area. At present (1975) the Plaza de Panama is bordered on the north by the following buildings: The West-Wing addition to the Fine Arts Gallery (1966) occupies the northwest side, the Fine Arts Gallery (1925) occupies the north side, and the Timken Gallery (1965) which occupies the northeast side. The southern part of the plaza has the House of Charm (1915) on the southwest side, and the House of Hospitality (1915) on the southeast.

In addition, the southern edge of the plaza is formed by the continuation of the El Prado arcade. There, on a traffic island in the middle of Pan American Road, is a monumental statue of El Cid on horseback. The statue is the work of Anna Hyatt Huntington and it was made in 1930.

The Plaza de Panama was originally part of the ground plan for the 1915 Panama-California Exposition.

Fine Arts Gallery

The Fine Arts Gallery is located on the northern edge of the Plaza de Panama. It is a two story building with a central portion that juts out slightly forward from two side east-west wings.

The side wings are roofed in red tile, in contrast to the central facade. In addition, the wings are mostly plain flat wall while the central facade is highly ornate.

The central facade is dominated by an elaborate portal.

The Fine Arts Gallery was designed by William Templeton Johnson and Robert W. Snyder. It was built in 1925. The building occupies the site of the Sacramento Valley Building from the 1915 Panama-California Exposition.

Casa del Prado

The Casa del Prado is located on the north side of El Prado, across from the Electrical Building. It is made up of two parts, the main building on El Prado, and a second building in the form of Mexican church, facing onto Village Place, an avenue which begins at the eastern end of El Prado and leads northward to Spanish Village. These two buildings are joined by an open arcaded courtyard.

(Continued)



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The interior of the main building is a two story arcaded courtyard, supported by plain rectangular pillars. This courtyard houses the Panama California Sculpture Court, in memory of Frank F. Evenson.

The Casa del Prado is a reconstruction of the original building that was built for the 1915 Panama-California Exposition. At that time it was known as the Varied Industries Building. In 1935, during the California Pacific International Exposition, the building was called the Food and Beverage Building.

Natural History Museum

The Natural History Museum is a three story concrete edifice located on the north-east corner of El Prado and Village Place. It is characterized by a central pavilion with adjoining eastwest wings, and a west wing addition extending northward.

The Natural History Museum was designed by William Templeton Johnson in 1931, and opened in 1933. The sculptural detail on the facade is the work of Arthur Putnam.

The site of the Natural History Museum was originally occupied by the Southern California Counties Building from the 1915 Panama-California Exposition. After the fair, the old building was renamed the Civic Auditorium. In December of 1925, the Civic Auditorium burned down. The site was marked for the new Natural History Museum which opened in January of 1933. In 1935, the building was called the Palace of Natural History during the California Pacific International Exposition. During World War II, the building was vacated and used as a hospital.

The Natural History Museum houses a superb collection, including the Klauber Rattlesnake Collection, the largest in the world. The building also houses the Klauber Herpetology Library, the largest collection of reptilian bibliography in the world."<sup>2</sup>

The circular Ford Building was built for the 1935 California Pacific International Exposition and is currently under restoration. Designed by Walter Dorwin Teague in the Art Moderne style, it is a fine example of the streamlined idiom with its rounded edges and geometrical details. Nearby is the Ford Bowl, (an amphitheatre) in the same style. They are sited on a promontory overlooking the city and the San Diego bay.

Most of the basic research on the Fair used herein is based as well on Theodore Prudon's paper for the SAH, Philadelphia, May 1976.

<sup>1</sup>Henderson, John, AIA Guide to San Diego.

<sup>2</sup>National Register of Historic Places nomination form.

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frequently in the arcades (with their pure circular arches), the administration building, the Cabrillo bridge, the botanical building and lily pond. In any event it is known that he acted as a consultant, but soon moved to Los Angeles, establishing a second office. Goodhue brought to San Diego the flamboyant, exciting Spanish-Colonial with the churrigueresque, the renaissance of the baroque and rococo styles, which turned the public taste away from the pure Mission simplicity. The Exposition plan was drawn, featuring the present well integrated classical scheme of landscaped buildings. The lath and plaster buildings were only to last a few years and were to be replaced later with permanent structures and landscaped gardens. The Exposition opened on 1 January 1915 and formally closed at the end of 1916, paying all its expenses and resulting in accomplishments that were far reaching for San Diego and its future role. Many new residents were attracted and, impressed by what they saw, stayed. Balboa Park emerged as the center of the city's social, cultural and recreational life. The San Diego Zoo was established (1922) using the few animals that had been exhibited. Art and cultural exhibits were utilized and formed the beginning of the Fine Arts Society, the Museum of Man and the Natural History Museum. Morley Field developed after John Nolan's plan in 1927 and many WPA projects were instituted in the 1930's. By 1935, the city was ready for another fair and the California-Pacific International Exposition was planned, with Richard S. Requa as architect. Requa, trained in Gill's office, had developed his own architectural vocabulary, solidified by his world travels and is responsible for the buildings in the Palisades area, the Spanish Village, Old Globe Theater, Alcazar Garden and the Cafe Del Rey Moro patio and gardens. Used by the military throughout the war, with little additional development, the park suffered its next greatest encroachment by freeway projects, Cabrillo Freeway in the 1940s and interstate 5, in the 1960s. Balboa Park, now 102 years old, has indeed weathered many storms and has been the focus of much of San Diego's development. Along with the harbor, Mission Bay and, once, Mission Valley (now tragically destroyed) it is still one of San Diego's greatest assets."<sup>1</sup>

Bertram Goodhue was a logical choice as a designer--he had traveled widely and, with Sylvester Baxter, wrote Spanish Colonial Architecture in Mexico. He had designed the Washington Hotel in Colon, Canal Zone and the Episcopal Cathedral in Havana. The master plan of the park was to be designed by the Olmsteds and Goodhue--the Olmsteds withdrew and Goodhue then created the park. With local architects working on individual structures.

(Continued)



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There are a few remnants remaining of America's Expositions--Memorial Hall in Philadelphia (1876), the Museum of Science Industry in Chicago, (1893), the empty meadows at Flushing--in San Diego one can see a great deal of the 1915 fair today.

<sup>1</sup>Henderson, John, AIA Guide to San Diego, 1976.

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Those portions of the land designated as "City Park Reservation," known as Balboa Park, in the Pueblo Lands of San Diego, in the City of San Diego, County of San Diego, State of California, according to Map thereof by James Pascoe, a copy of which was filed in the Office of the County Recorder of said County, and is known as Miscellaneous Map No. 36, which lies within Pueblo Lots 1135, 1143 and 1144 of the Pueblo Lands of San Diego, according to Map thereof by Charles H. Poole, a copy of which was filed in said County Recorder's Office and is known as Miscellaneous Map No. 35, described as follows:

Beginning at the intersection of the Easterly line of Route 163, as located and established as of the date of this instrument, with the North line of said Pueblo Lot 1135; thence Easterly along said North line of Pueblo Lots 1135 and 1136 to an intersection with the Westerly line of Park Boulevard, as located and established as of the date of this instrument; thence Southerly and Southwesterly along the Westerly and Northwesterly line of said Park Boulevard to an intersection with the Northerly line of Interstate 5, as located and established as of the date of this instrument; thence Westerly along said Northerly line to an intersection with the Easterly line of said Route 163, thence Northwesterly and Northerly along said Easterly line of Route 163, returning to Point of Beginning.

Said area shall encompass those structures which were erected for the Panama-California International Exposition of 1915-1916, the California Pacific International Exposition of 1935-36, and the Timken Fine Art Gallery and West Wing of the Fine Arts Gallery which were constructed in 1964.

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PHOTOGRAPHS AND THEIR DESCRIPTIONS

SITE: Balboa Park Historic District

LOCATION: San Diego, California

PHOTO CREDIT: City of San Diego

- DESCRIPTION:
- 1. Casa del Prado, view from South
  - 2. Casa del Prado, view from South
  - 3. The El Prado Arcade
  - 4. Natural History Museum, view from South
  - 5. Fine Arts Gallery, view from the South
  - 6. Electrical Building, view from the North
  - 7. House of Hospitality, view of the West
  - 8. House of Charm, North Portal
  - 9. Organ Pavilion, View from Northwest
  - 10. Cabrillo Bridge, view from the South
  - 11. Alcazar Gardens (partial view)

PHOTO CREDIT: HABS, Marvin Rand

- DESCRIPTION:
- 12. Balboa Park, Entrance
  - 13. Balboa Park, Entrance
  - 14. Botanical Garden
  - 15. Botanical Garden, Interior
  - 16. Botanical Garden



# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: January 21-22, 2004

Reference No.: 2.2a.  
Action Item

From: ROBERT L. GARCIA  
Chief Financial Officer

Prepared by: Gary R. Winters  
Division Chief  
Environmental Analysis

Ref: **SUMMARY – NOTICE OF PREPARATION OF AN ENVIRONMENTAL IMPACT REPORT, STATE ROUTE 163 IN SAN DIEGO COUNTY – IMPLEMENT CORRIDOR MANAGEMENT PLAN IN SAN DIEGO**

## ISSUE:

The California Transportation Commission (Commission) is being asked to review and comment at the January 2004 Commission meeting on the following Notice of Preparation (NOP):

- Route 163, KP 1.4/6.0 (PM 0.8/3.7). Implement the Corridor Management Plan to reduce the number and severity of median accidents on State Route 163 in San Diego.

## PROGRAMMING:

This document is the first step in the environmental process to implement the Corridor Management Plan to address several issues on Route 163 through Balboa Park. The plan includes features such as guardrail, slope and gore paving, lighting, signing and maintenance pullouts. The first project covered under this environmental document will be a Fiscal Year 2003-04 median barrier project in the State Highway Operation and Protection Program (SHOPP). The project will be amended into the SHOPP in January 2004. The total cost of the project is \$5,104,000.

## ALTERNATIVES BEING CONSIDERED:

- No-Build
- Variations in horizontal placement of median barrier

## POTENTIAL SIGNIFICANT ENVIRONMENTAL EFFECTS:

- Modifications and additions to the facility have the potential to change the character of two historic resources: 1) the Balboa Park Historic District; and 2) this portion of Route 163, which is designated an Historic Parkway and is listed on the National and California Registers of Historic Places and is an historic landmark of the City of San Diego.

**PROPOSED MEASURES TO MINIMIZE HARM:**

- Enhance and upgrade the irrigation system, ground cover and shrubs.
- Plant trees in the median and at two proposed adjacent locations.
- Execute a Cooperative Agreement for additional planting outside the State right of way.
- Age the replacement concrete to match the concrete from the original construction in the Historic District.
- Keep new safety signage to a minimum.
- Where possible, conceal new traffic monitoring stations behind guardrail or under bridges.
- Replace fencing with new vinyl coated fencing to assist in visual integration.
- Remove unnecessary pavement and plant trees.

Attachments



# NOTICE OF PREPARATION

To: \_\_\_\_\_  
(leave blank—will be filled in with Responsible/Trustee Agency)

From: California Dept. of Trans. MS -46  
P.O. Box 85406  
San Diego, CA 92186-5406

**Subject: Notice of Preparation of a Draft Environmental Impact Report**  
*Reference: California Code of Regulations, Title 14, (CEQA Guidelines) Sections 15082(a), 15103, 15375.*

Project Title: State Route 163 Corridor Management Plan

**Project Location: State Route 163 in San Diego County from "A" Street through the Balboa Park historic district to the Sixth Avenue Undercrossing.**

Project Description: The proposed project is aimed at minimizing the number and severity of accidents that occur in the median. Upgrading and enhancing the safety, scenic quality and historic resources on approximately three miles [PM 0.8–3.7 (KP 1.4-6.0)] of the State Route 163 corridor through Balboa Park.

This is to inform you that the California Department of Transportation will be the Lead Agency and will prepare an environmental impact report for the project described below. Your participation as a responsible agency is requested in the preparation and review of this document.

We need to know the views of your agency as to the scope and content of the environmental information that is germane to your agency's statutory responsibilities in connection with the proposed project. Your agency will need to use the EIR prepared by our agency when considering your permit or other approval for the project.

A more detailed project description, location map, and the potential environmental effects are contained in the attached materials.

**A copy of the Initial Study (  is ) (  is not ) attached.**

Due to the time limits mandated by State law, your response must be sent at the earliest possible date but **not later than 30 days after receipt of this notice.**

Please direct your response to Jason A. Reynolds, Environmental Analysis, Branch A, Chief at the address shown above. Please supply us with the name for a contact person in your agency.

Date \_\_\_\_\_

Signature \_\_\_\_\_  
Title \_\_\_\_\_  
Telephone \_\_\_\_\_

# SR-163 Environmental Impact Report

EA 08140K

KP: 1.4-6.0

## PROJECT DESCRIPTION

The primary objective of these proposed projects is to lessen the number and severity of accidents that occur in the median. These projects would implement the State Route 163 (SR-163) Corridor Management Plan (CMP) developed by the California Department of Transportation (Department) in response to continued safety concerns and the recommendations of the SR-163 Safe and Scenic Advisory Committee (SSAC) to upgrade the safety within this corridor.

The proposed project covers a variety of features aimed at upgrading the safety of the freeway, enhancing the scenic views while preserving and restoring historic elements of the facility.

The following are a list of activities and features that are proposed for implementation and would meet the purpose and need of the SR-163 Corridor Management Plan.

### FEATURES

**Guardrail:** The main safety element is the proposed installation of dual runs of median guardrail in order to shield trees in the median from being struck by errant vehicles leaving the roadway. Of the proposed projects to be implemented along the SR-163 corridor, the installation of guardrail is the most controversial feature.

**Landscape:** Proposed installation of new trees that have not been replaced over approximately the past 25 years, removal of dead or dying trees and removal of non-native species within the freeway corridor. The current deteriorated condition of the landscape is causing visual impacts. Trees are dying due to the infestation of the Lerp Psyllid, a tree parasite.

**Irrigation:** Proposed upgrade of the irrigation system to current Department standards with Remote Irrigation Control Systems and bubbler type irrigation heads to help eliminate waste, saving water and money. The irrigation system is old and is being replaced in kind.

**Walls:** Evaluation and rehabilitation of crib walls and retaining walls that have been in place since the original construction of the freeway. There are two retaining type walls that are currently in a state of decline, with rebar showing through the outside of the concrete. These walls would be refurbished or replaced in kind, depending on the



evaluation by maintenance, and the new concrete would be aged to match concrete from the original wall construction within the Historic District.

**Slope and Gore Paving:** Evaluation and rehabilitation of gore areas and slope areas, especially slopes under bridges, where deterioration is a significant factor in the looks and condition of the existing paving. Currently, no plants or vegetation grow on slopes under the bridges. The proposed project would pave these slope areas and the concrete would be aged to match concrete from the original highway construction. This would blend with the existing surroundings after completion. **The impacts on the Historic District would be significant because of the change in the character of the resource.** The proposed project would remove vegetation and shorten the raised portion of the gore points and restripe the lanes after completion.

**Lighting:** Proposed changes to the type of illuminating bulbs contained in the lighting standards from low-pressure sodium fixtures to 310-watt high pressure sodium fixtures. This will be consistent with fixtures used by the City of San Diego south of Interstate 8.

**Traffic Signing:** Increase safety provided by upgrading signage to current Department standards by including reflective signs, warning signs, and adding exit numbering. Signage would be kept to a minimum within the Historic District to minimize the changes in the character of the resource.

**Traffic Monitoring Stations:** Proposed upgrade to the existing and installation of new Traffic Monitoring Stations in order to relay current traffic conditions to the Transportation Management Center which monitors speeds and flow of traffic. Even though they are small additions, they should be concealed behind guardrail or under bridges to minimize visual impacts.

**Fencing:** Replace existing fencing due to significant and severe deterioration and lack of proper placement. The fencing, in some places, is outside the Department right of way and in other locations it is inside the right of way. The fencing will be replaced in kind, and will not cause a significant change to the character of the Historic District.

**Maintenance Vehicle Pullouts:** Installation of safe pullout areas for personnel and vehicles while maintenance activities are being conducted. It would no longer be necessary to close lanes and cause impacts to traffic flow through the corridor. This element would be an addition to the Historic District and cause a change in the character of the resource.

**Abandoned Equipment and Appurtenances:** Currently, there are areas of pavement that had previously been used for on or off ramps on SR-163 or I-5. In these areas, proposed pavement removal and installation of trees is proposed.

## **TRAFFIC**

The SR-163 corridor has experienced an increase in the volume of traffic, which has led to a high accident rate, fatal accidents in particular. The information found in the Traffic Accident Surveillance and Analysis System (TASAS) and the Median Barrier Monitoring Program (MBMP) have demonstrated that the lack of safety features and additional traffic trips have added to the increased number and severity of accidents. The lack of a median guardrail has added to the severity of these accidents since there is no shielding of median trees. Errant vehicles hit the trees in the median when they leave the roadway. The percentage of fatal accidents on SR-163 through Balboa Park is higher than the state average for similar facilities.

SD -163 - 1.2/2.4 Accident Summary (Nov 1997 - Oct 2002)			
Accident Type	3-Year	4-Year	5-Year
Total # of Accidents	167	250	316
Fatal (# of incidents)	1	3	5
Injury	55	79	95
PDO	111	168	216
Killed (# of persons)	1	4	6
Injured	82	125	156
PDO = Property Damage Only			

A second major safety issue is that of maintenance workers who must accomplish trash removal, tree trimming and weed removal along the corridor. There is a current lack of accessibility to maintain the existing vegetation. Maintenance accomplishes these tasks by doing a complete closure of the highway. The highway closure is usually done on a Sunday morning in one direction of traffic and all of the maintenance crews tasks are completed at one time approximately once a quarter.

The general condition of the vegetation along the SR-163 corridor has been in a state of decline. Eucalyptus trees, planted during the early 1900's, have been attacked by Lerp Psyllid infestation and many of the original trees have died. This infestation of the Eucalyptus forest has been devastating to the viewshed. There has been a long-standing Department policy of not replacing median trees due to the safety concerns regarding lack of shielding. As one drives through the existing corridor, you are met with brown areas in the landscaping on the shoulders and outside of the State right of way. This condition is considered a high visual impact.

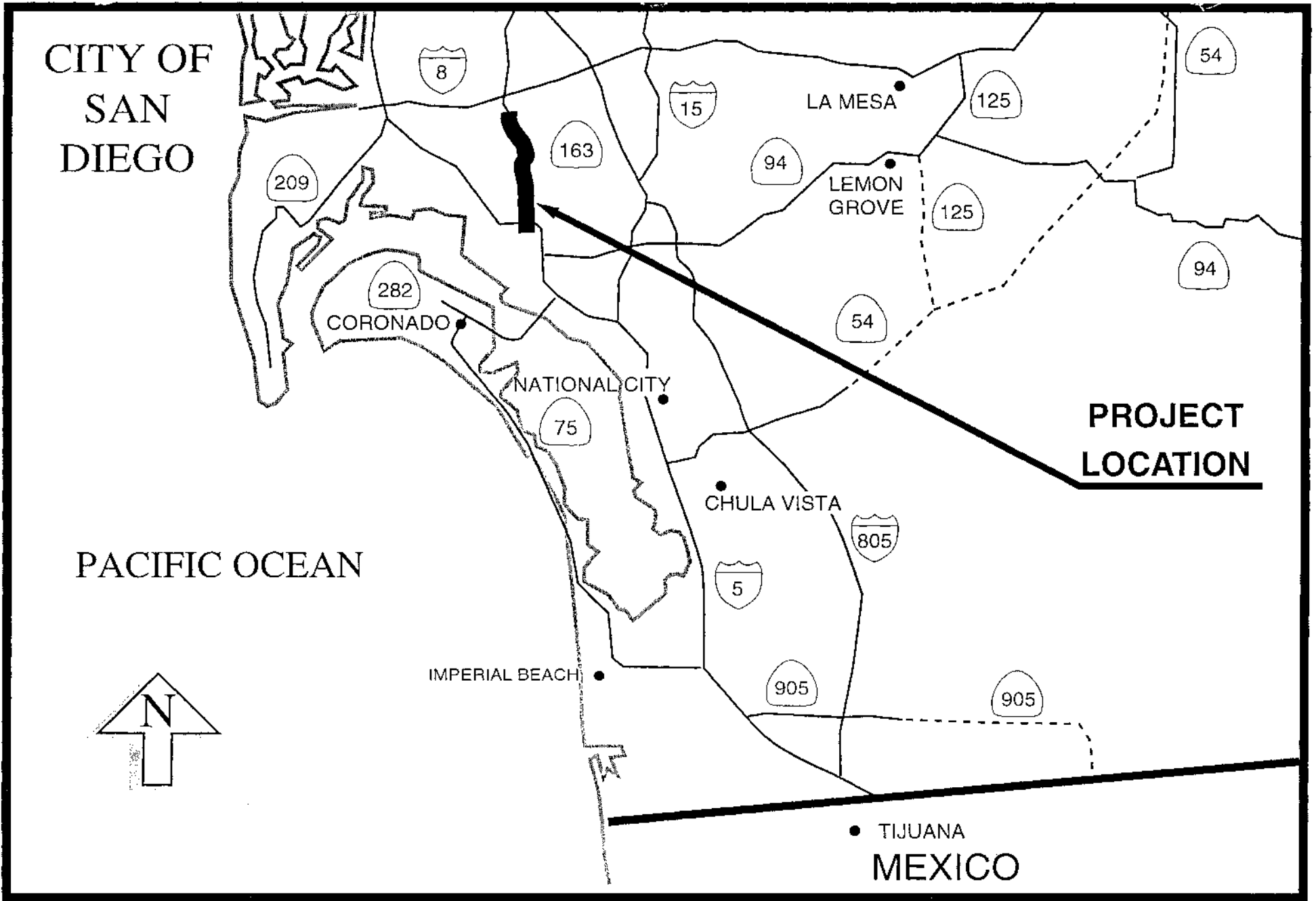
The steady deterioration of the vegetation over the years has prompted the need for enhancement and upgrading the irrigation system, ground cover and shrubs within the State right of way. Of the projects listed above, the installation of the guardrail would be the highest visual impact and proposed mitigation for this would be tree planting in the median. A Cooperative Agreement between the Department and the City is being proposed for additional mitigation planting out side of the State right of way.

The portion of SR-163 that traverses Balboa Park has also been designated an Historic Parkway, the Cabrillo Historic Parkway (AB 3025), and is listed on the National and California Registers of Historic Places as well as an historic landmark by the City of San Diego.

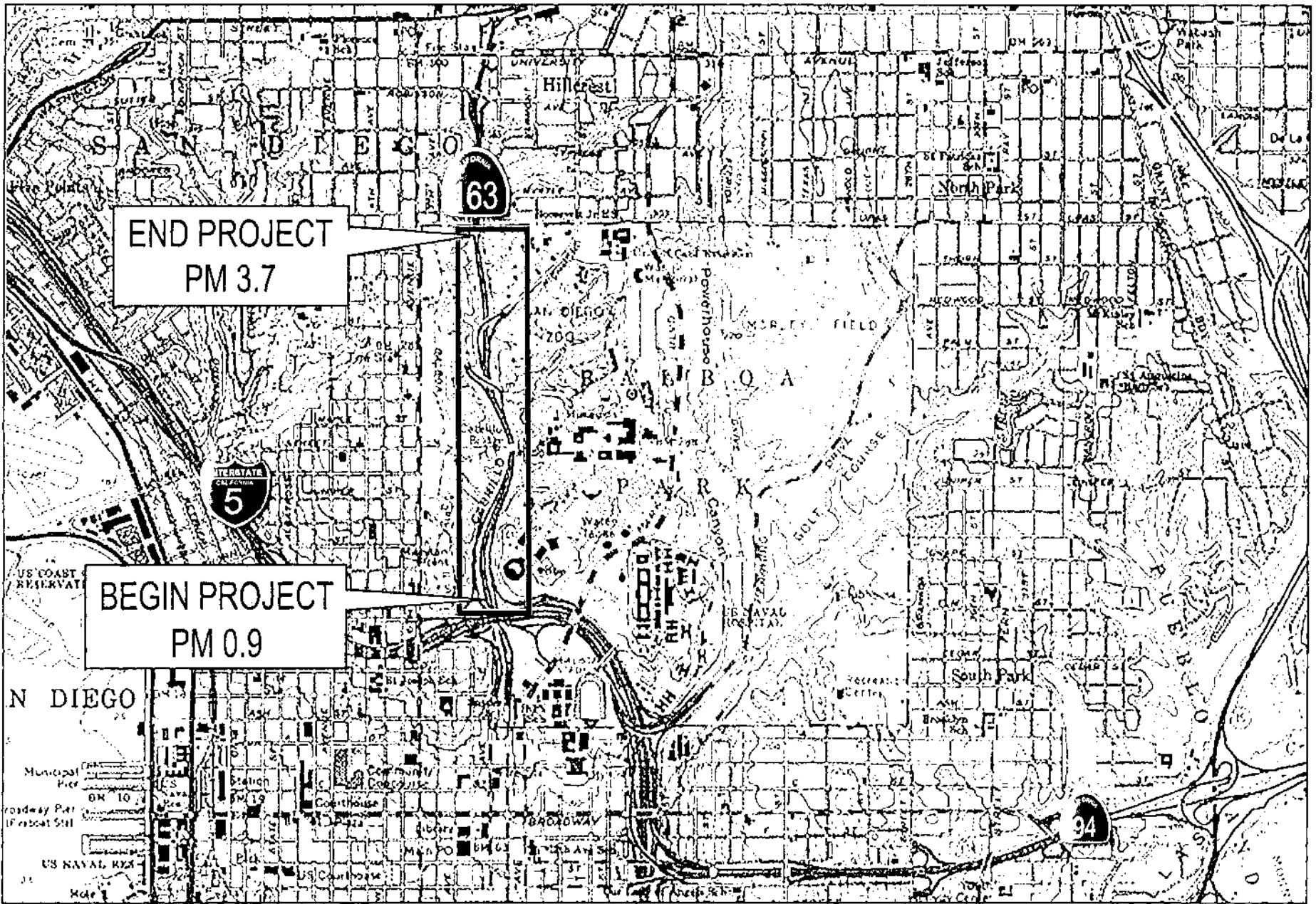
## **FUNDING**

The funding for the projects associated with the CMP would come from State only funds, specifically, the State Highway Operation and Protection Program. This program includes funds for Rehabilitation and Safety projects and non-capacity increasing construction projects. In order to accomplish the various projects, these tasks are scheduled to be completed in phases over approximately a 10-year period.

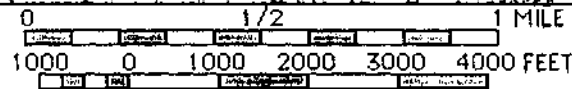




**PROJECT LOCATION MAP**



TN \* MN  
13 1/2°



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SR-163  
E.A. 08140K

PROJECT LOCATION MAP

Appendix B - January 8, 2011 Page 15

# Memorandum

To: CHAIR AND COMMISSIONERS

CTC Meeting: February 25-26, 2004

Reference No.: 2.2b.(2)  
Action Item

From: ROBERT L. GARCIA  
Chief Financial Officer

Prepared by: Gary R. Winters  
Division Chief  
Environmental Analysis

Ref: **SUMMARY – DRAFT ENVIRONMENTAL IMPACT REPORT, STATE ROUTE 163 IN SAN DIEGO COUNTY – IMPLEMENT CORRIDOR MANAGEMENT PLAN IN SAN DIEGO**

## ISSUE:

The California Transportation Commission (Commission) is being asked to review and comment at the February 2004 Commission meeting on the following Draft Environmental Impact Report (DEIR):

- **Route 163, KP 1.4/6.0 (PM 0.9/3.7). Implement the Corridor Management Plan to reduce the number and severity of median accidents on State Route 163 in San Diego.**

## PROGRAMMING:

This document is the first step in the environmental process to implement the Corridor Management Plan to address several issues on Route 163 through Balboa Park. The plan includes features such as guardrail, slope and gore paving, lighting, signing and maintenance pullouts. The first project covered under this environmental document will be a Fiscal Year 2003-2004 median barrier project in the State Highway Operation and Protection Program (SHOPP). The project was amended and fully funded in the proposed 2002 SHOPP in January 2004. The total capital cost of the project is \$4,450,000 and is on the FY 2003/04 SHOPP Allocation Plan.

## ALTERNATIVES BEING CONSIDERED:

- No-Build
- Variations in horizontal placement of median barrier



**POTENTIAL SIGNIFICANT ENVIRONMENTAL EFFECTS:**

- Modifications and additions to the facility have the potential to change the character of two historic resources: 1) the Balboa Park Historic District; and 2) this portion of Route 163, which is designated an Historic Parkway, is listed on the National Register of Historic Places and the California Register of Historical Resources, and is an historic landmark of the City of San Diego.

**PROPOSED MEASURES TO MINIMIZE HARM:**

- Enhance and upgrade the irrigation system, ground cover and shrubs.
- Plant trees in the median and at two proposed adjacent locations.
- Execute a Cooperative Agreement for additional planting outside the State right of way.
- Age the replacement concrete to match the concrete from the original construction in the Historic District.
- Keep new safety signage to a minimum.
- Where possible, conceal new traffic monitoring stations behind guardrail or under bridges.
- Replace fencing with new vinyl coated fencing to assist in visual integration.
- Remove unnecessary pavement and carry out tree planting.

Attachments

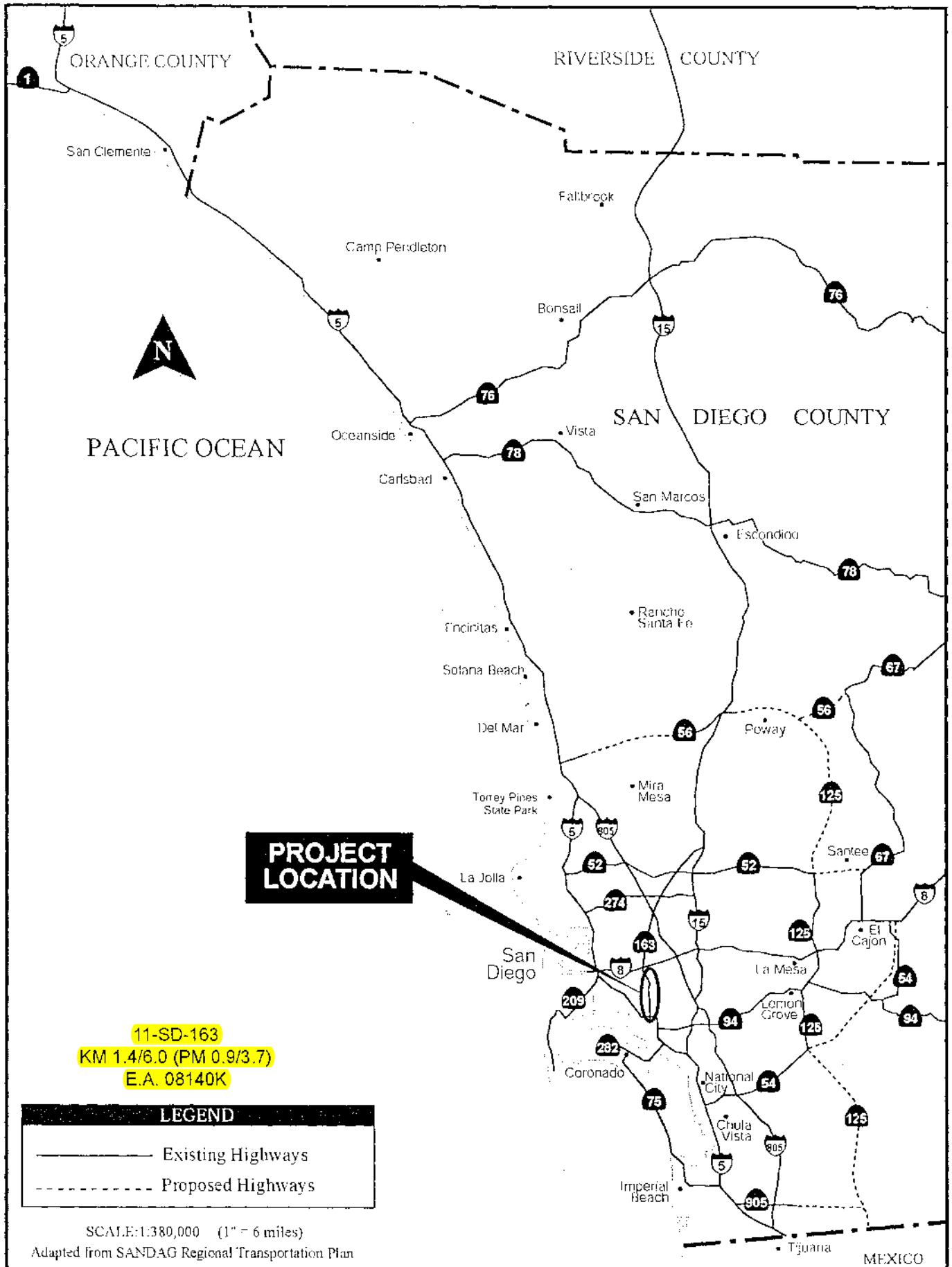


FIGURE 1  
 PROJECT LOCATION MAP

## EXECUTIVE SUMMARY

This Draft Environmental Impact Report (DEIR) evaluates a reasonable range of project alternatives, assesses the individual and cumulative impacts resulting from the proposed project, identifies measures to minimize or avoid significant impacts, and presents the associated mitigation measures. A Corridor Management Plan (CMP) for State Route 163 (SR 163) was prepared by the California Department of Transportation – District 11 (Department). The CMP developed a coordinated and programmatic approach to address needed safety improvements, corridor preservation and enhancement efforts on SR 163. The CMP is being used as a guide for the preparation of this DEIR.

### Purpose and Need

The purpose of the proposed project is to reduce the severity of median accidents on SR-163 between KP 1.4-6.0 (PM 0.9-3.7), enhance the scenic quality of the corridor, and to preserve and restore historic elements of the corridor. The fatal accident rate from KP 1.9-3.8 (PM 1.2-2.4) is 0.022 fatal accidents/per million vehicle miles/per year, which is higher than the statewide average of 0.016 fatal accidents/per million vehicle miles/per year and exceeds the Department's accident criteria warrants. The corridor is experiencing a deterioration of landscaping, irrigation systems, fencing, and retaining walls. The corridor also has a number of non-standard features, including slope paving.

This corridor is completely absent of dedicated maintenance access points and because of high traffic volumes, requires the complete closure of either southbound or northbound lanes for any substantial maintenance activities. Maintenance personnel must be provided with a safe environment for their associated activities while preserving the free-flow of vehicular traffic. The elements of this proposed project are designed to address each of the stated deficiencies and needed improvements.

### Proposed Project Elements:

- Construct Median Barrier.
- Rehabilitation and Restoration of the Landscaping.
- Replace and Upgrade Traffic Monitoring Stations (5 locations)
- Replace Irrigation System.
- Replace and Upgrade Upas Street Bridge Sign.
- Rehabilitate Robinson Avenue Retaining Wall.
- Introduce Slope Paving (Seven Locations).
- Construct Maintenance Vehicle Pullouts (Six Locations).
- Remove Paving and Landscape Abandoned Ramp (I-5/SR-163 interchange).
- Rehabilitate Existing Maintenance Access Road.
- Plant trees within the Balboa Park viewshed.



The CMP studied two additional elements that would not be implemented as part of this proposed project:

- Modification to Lighting Standards
- Bridge Rail Replacement
- Pavement Removal and Landscaping of the Abandoned Quince and Richmond Street Ramps.

### **Timing of Project Elements**

Construction of the median barrier and restoration of the median landscaping would be the first order of work (2004) following the anticipated approval of this DEIR. The remaining elements would be implemented over an approximate ten-year period, depending upon available funds. The year of anticipated action for the remaining elements is indicated in the Impact Matrix. (Table ES-1)

### **Median Barrier Alternatives**

A number of alternative barrier types and barrier locations have been analyzed in conjunction with preparation of the CMP. The barrier types considered include:

- Concrete Barrier (various types, heights and locations)
- Thrie Beam (Metal Face Rail)
- Steel Backed Timber Guardrail (Timber Posts and Rails)

All of the barrier types studied, except Steel Backed Timber Guardrail, were withdrawn from consideration in response to public opposition and the lack of integration with the visual and historic resources of the corridor.

The Steel Backed Timber Guardrail (SBTG) is proposed within the median at two alternative horizontal placement locations, either 0.6 meter (2 feet) or 1.2 meters (4 feet) from the edge of the traveled way (existing concrete pavement).

### **Project Elements Alternatives**

The only alternative to the remaining elements of the proposed project is the No Project Alternative. As indicated above each of these elements is proposed to address a needed safety improvement, restore a deteriorated feature, or preserve and enhance the visual and historic resources of the corridor.

### **Summary of Impacts**

The proposed barrier introduces a new visual feature into the median that has historically been clear of any man-made structure. There are no impacts from the proposed project that would remain significant after mitigation. The proposed median barrier would have visual and historic impacts that would be mitigated to a level below significance. This new feature would have an impact on the cross-median views and would change the historic context of this segment of SR 163.

The median barrier would not detract from the historic significance of the corridor, and would not jeopardize the corridor's listing on the local, state, and federal registers. The proposed mitigation measures include:

- The type of barrier selected, SBTG;
- The restoration of median landscaping and recordation of the SR 163 historic corridor in accordance with standards established by the U.S. Department of the Interior, National Park Service; and,
- Offsite planting at two proposed locations the Marston Addition and the Archery Range. (See Figure 17)

The SBTG is proposed as a measure that balances visual integration while also addressing an important safety element of the corridor. The re-introduction of landscaping into the median provides for visual mitigation as well as helping to restore historic features of the corridor.

Three elements of the proposed project, slope paving, maintenance vehicle pullouts (MVPs), and abandoned ramp removal at the I-5/SR-163 interchange, present impacts that are only considered significant prior to mitigation in a cumulative context. The slope paving would add new paving under existing structures. Mitigation for these new or modified features would include concrete that has been colored and given an aged appearance to match the existing conditions. The proposed MVPs would be strategically placed to minimize their appearance in the corridor. The pavement removal and introduction of new landscaping at the abandoned ramp location would enhance the visual quality of the corridor.

Additional elements discussed above and proposed for future implementation, which do not have the potential for significant impacts individually or cumulatively to the visual and historic resources of the corridor include:

- Replacement of the Irrigation System
- Replacement of Fencing
- Replacement/Introduction of Traffic Monitoring Stations
- Retaining Wall Rehabilitation at Robinson Avenue
- Maintenance Access Road

### **Areas of Controversy**

The proposed placement of anything new, such as a median barrier, within this portion of SR 163 has been met with public concern and opposition. The Department has conducted extensive outreach efforts in an attempt to resolve these concerns and the opposition raised. The CMP was prepared in direct response to some of the concerns voiced by the public and stakeholders. The proposed barrier type selected, Steel Backed Timber Guardrail, stems from these outreach efforts. The communication and coordination with the public is ongoing and would continue throughout the project development and implementation processes.

### Additional Department Projects

In 2001, the Department approved a pavement rehabilitation project on SR 163. This project was reviewed and approved independently of the proposed project presented in this DEIR. The pavement rehabilitation project includes:

- Rehabilitating the main lanes and shoulders.
- Installing rumble strips along the inside shoulders.
- Widening shoulders at gore points and near bridge columns.
- Curb relocation at bridge columns.
- Minor drainage inlet modifications.
- Upgrade existing metal beam guardrail installations and end treatments.
- Reconstruction of up to 0.6 m (2 ft) of paved shoulder with relocation/upgrade of median irrigation system.
- Upgrade existing signs with retro-reflective material.
- Rehabilitate pavement of selected ramps.
- Gore Paving/Rehabilitation

This project is scheduled to begin construction in 2004.

The Department has also approved and completed a traffic signage upgrade project in 2003.

### Actions by Others

The San Diego Zoo has proposed a project, the Park Boulevard Promenade, which includes the construction of a new employee parking lot to accommodate 450 parking spaces. Access to the parking lot would be from Richmond Street using an existing zoo service driveway (City of San Diego Draft Environmental Impact Report for the Park Boulevard Promenade, page 3-26).

The San Diego Padres are nearing completion of a new ballpark in the Center City East area of downtown. Patron access to and from this new sports facility will introduce additional traffic to SR 163. Although the ballpark would present no direct visual impacts to the SR 163 corridor, the anticipated increase in traffic further supports the need for addressing the existing non-standard features within the corridor.

### Permits, Reviews, & Approvals Required

The proposed mitigation areas, the Marston Addition and the Archery Range, as well as, the maintenance access road proposed for rehabilitation are under City of San Diego ownership. The Department is currently developing a Cooperative Agreement with the City of San Diego for access to the proposed mitigation sites and to rehabilitate the maintenance access road. Access to the highway facility begins at the Department right-of-way at an existing gate from that point the access road is within the identified mitigation area. Approval of this Cooperative Agreement, between the Department and the City of San Diego, would be critical to implementing some of the proposed mitigation measures and providing maintenance personnel with a dedicated access point. The Cooperative Agreement would define the need for three years of maintenance by the Department, and afterwards, the City of San Diego would resume control of the management of



the landscaping. A Maintenance Agreement and a Right-of-Way Agreement with the City of San Diego would be necessary for permanent access.

The Notice of Preparation (NOP) for this DEIR has been submitted to the State Clearinghouse on November 25, 2003. The next CTC action would involve a vote approving the currently proposed project, approving the funding source, and certifying the Final Environmental Impact Report, anticipated for April 2004.

Concurrence from the State Historic Preservation Officer (SHPO) on the Historic Resources Compliance Report (HRCR) has been requested. The SHPO concurrence will be included in the Final EIR.

Actions by the California Transportation Commission will include filing a Notice of Determination for the EIR and approving project funding.

**Table ES-1: Summary of Potential Impacts**

PROJECT ACTION	IMPACTS	COMPENSATION/MITIGATION	SIGNIFICANCE	APPROXIMATE YEAR OF IMPLEMENTATION
Construct Median Barrier	A new element within the Scenic Corridor and the Historic District.	Use of SBTG. New tree planting in the median. Historic American Engineering Record (HAER) documentation.	Mitigated below significance.	2004
Install/Upgrade Irrigation System	Temporary visual impacts during construction and plant re-establishment.	New planting and ground cover at construction locations. Replacement in kind of a non-contributing element to the Historic District.	No significant impacts.	2004
Replace Fencing	Temporary visual impacts during construction.	Replacement in kind.	No significant impacts.	2005
Construct Maintenance Vehicle Pullouts (MVPs) (6 locations)	A new element within the Scenic Corridor and the Historic District.	Locate to minimize visual impacts, pave with asphalt, keep number of locations at six or fewer..	Mitigated below significance.	2006
Refurbish Retaining Wall	Restoring an existing deteriorated wall.	Replacement in kind.	No significant impacts.	2007
Slope Paving (7 locations)	A change in the character of the resource.	New visual element, paving under bridges which would be treated to match the aged appearance of existing concrete from original construction. Replacement in kind.	Mitigated below significance.	2008
Signs	Installing Vertical Clearance Sign on Upas Street Bridge.	Photo documentation/recordation prior to sign installation.	Mitigated below significance.	2004
Remove Pavement at Abandoned Ramp (I-5/SR-163 Interchange)	Temporary visual impacts during construction.	Tree planting at pavement removal location.	Mitigated below significance.	2009
Install Traffic Monitoring Stations	No impacts.	None.	No significant impacts.	2011
Upgrade Maintenance Access Road	A change in the character of the resource.	Restore landscaping to minimize the visual impacts and shield from the traveling public.	Mitigated below significance.	2006